



THE CITY OF  
**COLUMBUS**  
ANDREW J. GINTHER, MAYOR

DEPARTMENT OF  
PUBLIC SERVICE

# INDIANOLA AVENUE COMPLETE STREET STUDY

# Public Engagement Report

October 29, 2021

Prepared by:



Warhol & WALL ST.  
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## Project Overview/Goals

- With a planned resurfacing project scheduled to occur in late 2023 on Indianola Ave, the goal for this project is to incorporate complete streets elements into the planned project such as reallocating travel lanes to other transportation modes, improving safety, and reducing crashes.
- The goal of this project is to address multimodal connectivity and safety through examining:
  - Create a design that encourages motorists to drive at safe speeds
  - Improve the pedestrian experience
  - Create better bikeway connections
  - Accommodate safe and reliable transit service
  - Develop a parking strategy that reasonably meets the needs of businesses, patrons, and residents

This plan is being led by The City of Columbus, Michael Baker International with outreach and engagement led by Warhol & WALL ST.

## Public Engagement Scope/Goals

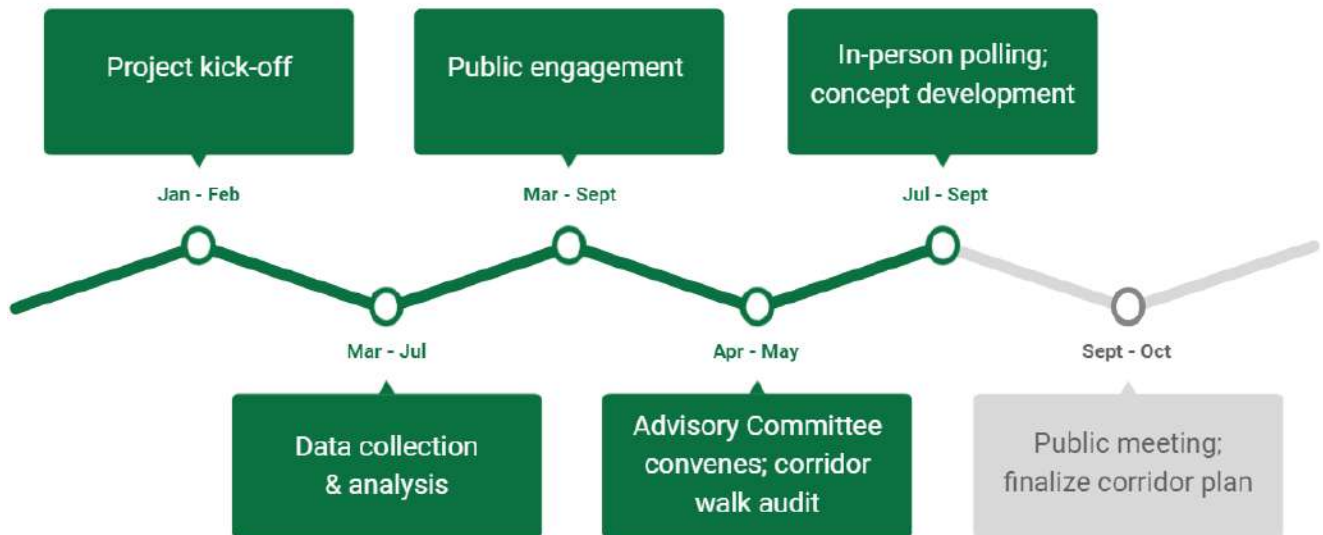


Figure 1. Project Timeline

Warhol & WALL ST. led the public involvement efforts, with a goal to create a detailed, collaborative and highly resident-centric public involvement plan to ensure that the project not only enhances the multimodal goals of the City, but also achieves the goals of local stakeholders who may be impacted. The aim was to empower residents with information throughout the project and lead all stakeholders on a collaborative journey to achieve mutual goals.

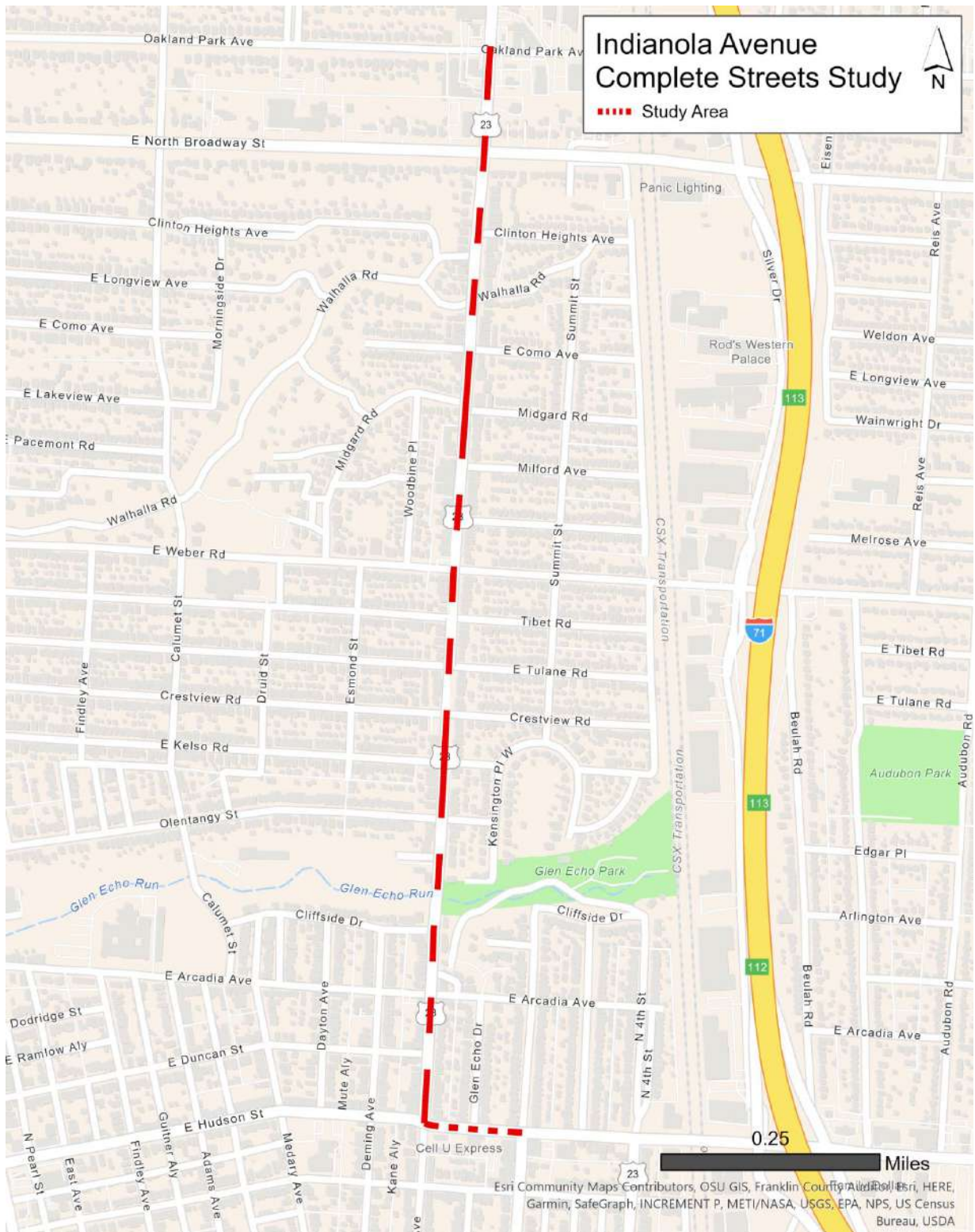


Figure 2. Indianola Avenue Complete Streets Study Area Map

## Public Engagement Tactics

**Stakeholder Discovery:** Gathering data to compile a creative brief allows us to fine-tune a strategy that is constituent-focused, creating the right branding messages and prompts that will motivate the stakeholders towards engagement.

**Community Engagement (Advisory Committee Recruitment and Convening):** Identifying key stakeholder groups to be engaged as an advisory committee will be necessary before advancing to the general public involvement. The following tactics highlight some of the primary ways that stakeholders will be engaged throughout this process:

- Virtual public information sessions for the community to attend and receive information about the project
- Discovery sessions with the City, and Michael Baker Design Team to create an intentional and inclusive advisory committee that will help guide our collective goals
- Organizing and convening an advisory committee of relevant local stakeholders to guide the planning process and the public involvement/engagement plan

**Engagement Design and Collateral Production:** Working with the City to ensure that our public awareness campaign aligns with their stakeholder outreach goals, we will develop Key Performance Indicators to evaluate the success of the engagement tactics. Creating content via presentations, engaging community events, and properly messaged digital content may be used to engage and gain feedback from our advisory team and additional stakeholders.

**Public Involvement/Marketing Plan:** The final comprehensive public involvement plan will outline the means and methods to inform and educate customers throughout the entirety of the multimodal project. Messaging workshops will ensure that we have clarity and alignment in all communications with constituents. This plan will take into consideration the unique needs of the different communities and neighborhoods who will be impacted by the work, including considerations for demographics, language, and culture.

## Advisory Committee

The Advisory Committee is an informal group of residents, business owners and other community stakeholders engaged in the Indianola community who were consulted and informed about plan developments. As an informal committee, member expectations included:

- Attending meetings to give input and advice when needed
- Sharing information and surveys with your friends and neighbors in the community
- Inviting key people to share their voice with us

## Advisory Committee Members

- Clintonville Area Commission
  - District 1 – Dave Vottero, [cacdistrict1@gmail.com](mailto:cacdistrict1@gmail.com)
  - District 3 – Libby Wetherholt, [libbywD3@gmail.com](mailto:libbywD3@gmail.com)

- District 4 – Judy Minister, [judym.cac4@gmail.com](mailto:judym.cac4@gmail.com)
- Chair – BJ White, [bjwhite.cac.district9@gmail.com](mailto:bjwhite.cac.district9@gmail.com)
- Planning and development committee – Andrew Overbeck, [aoverbeck@gmail.com](mailto:aoverbeck@gmail.com)
- University Area Commission
  - District 1/Planning and transportation committee – Lauren Squires, [lauren.squires@gmail.com](mailto:lauren.squires@gmail.com)
  - Andrew Hinger, [andrewjhinger@gmail.com](mailto:andrewjhinger@gmail.com)
  - Daniel Snider, [danielsnider.1@gmail.com](mailto:danielsnider.1@gmail.com)
  - Chair – Doreen Uhas-Sauer, [doreencolumbus@icloud.com](mailto:doreencolumbus@icloud.com)
- Commercial/businesses
  - The Crest Gastropub - Ali Alshahal, Owner, [ali@arcreativegroup.com](mailto:ali@arcreativegroup.com)
  - The Crest Gastropub - Abed Alshahal, [abed@arcreativegroup.com](mailto:abed@arcreativegroup.com)
  - Savor Grownl – Faris Habil, Owner, [savorgrownl@gmail.com](mailto:savorgrownl@gmail.com)
  - Studio 35 – Eric Brembeck, [Studio35llc@yahoo.com](mailto:Studio35llc@yahoo.com)
- Interest groups/residents
  - COTA – Corey Francis, [FrancisCM@cota.com](mailto:FrancisCM@cota.com)
  - Yay Bikes! – Erin Synk, [esynk@lnegroup.com](mailto:esynk@lnegroup.com)
  - MORPC – Stephen Patchan (also a resident), [spatchan@morpc.org](mailto:spatchan@morpc.org)

Meeting Name	Date	Location	Participants
Indianola Advisory Committee Meeting 1	April 15, 2021	Virtual	Advisory Committee
Walk Audit - Indianola Ave.	May 20, 2021	In-Person	Dave Vottero - CAC Emma Kogge - City Stephen Patchan - MORPC Amber McNair - OSU Colleen - Resident Andrew Overbeck - CAC Eric Brembeck - Studio 35 Kevin Dickens - Michael Baker Josh Vidmar - Michael Baker Tahja Lauderdale - W&W Corey Francis - COTA Tricia Fought - City Ariane May - City
Man on the Street Activation	July 9 – 10, 2021	In-Person	Emma Kogge Justin Goodwin Libby Wetherholt Dave Vottero
Indianola Advisory Committee Meeting 2	August 31, 2021	Virtual	Advisory Committee
Public Meeting	September 16, 2021	Virtual	Open to the Public



## Advisory Committee Meeting #1

**Purpose:** To kick off the Indianola Avenue Complete Streets Study and begin the engagement of the advisory committee

### Key Takeaways:

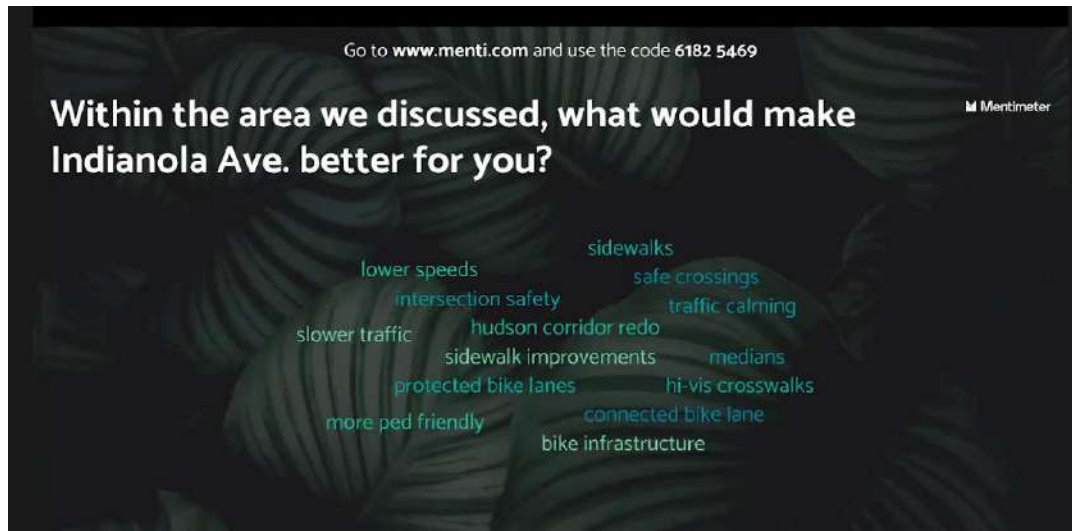
- The sidewalks on Indianola Avenue are considered in poor repair and unfriendly to groups of pedestrians.
- Biking, walking and driving are all used to get around the street.
- Advisory committee members with children are disappointed with the lack of walkability in this neighborhood, citing fear of vehicular accidents while walking or biking.
- The intersection at Hudson and Indianola is considered dangerous and needs to be reconfigured for intuitive pedestrian safety.
- A crosswalk or crossing light is desired at Walhalla Road, Cliffside Drive and Weber.
- Between Weber and Arcadia, there is nothing to stop traffic or monitor speed. Slower traffic is seen as a need throughout the street.
- There is a desire to fill the missing gap in the bike network.
- Reaching a parking solution that will satisfy everyone who works and lives on Indianola will be a fine balancing act.

**Meeting Comments & Summary:** Our meeting attendees were engaged and excited about the future of Indianola Avenue. The chat was lively and comments were insightful.

- Pedestrian safety is a major concern on Indianola Avenue.
- Resident Lauren Squires said she felt unsafe walking with her children down the street. Sidewalks are hard to navigate and not handicap accessible. Attendees were enthused about the projected crosswalks, with thoughts that motorists and poor traffic slowing measures make the street unsafe to cross. There was specific concern that the poor sight lines and sidewalk condition make the Indianola and Hudson intersection unsafe. Connecting the bike network and improving bike infrastructure (such as adding a protective median like on Summit Street) were also high priorities. Residents intuitively feel that the neighborhood should be walkable, and are disappointed that it is not.

Several committee members feel that removing street parking may be a worthy investment if it makes the street more accessible, because residential parking is well accommodated. If street parking is altered on Indianola, it should not be framed as taking parking away; instead the focus should be placed on the potential to create a sustainable and walkable business district and add new biking infrastructure. However, local business owners were concerned about the negative effects minimizing street parking would have on their businesses. They requested that parking not be removed from stretches of the road surrounding businesses and that parking only be altered on the East side of the street. There is also interest in parking on the currently-prohibited Cliffside Drive bridge.

## Live Polling Results



## Advisory Committee Meeting #2

**Purpose:** To update Advisory Committee members on project deliverables and provide information on initial concept alternatives

**Key Takeaways:**

- Planning for the safety of bicyclists and pedestrians using the corridor is important to residents
- Some concern over parking reductions to accommodate other facilities
- Need to fill the bikeways gap was shown in the CAC mobility survey

**Meeting Comments & Summary:** Our meeting attendees included members of the Advisory Committee plus several corridor-area residents. Their insights into the project included:

- Connections between the proposed and existing bike infrastructure (i.e. to the Summit Street cycle track)
  - Arcadia to Summit connection
  - Summit St. alternate route
- Safety improvements proposed with the plan and effects on pedestrians and bicyclists
- Comments surrounding parking spaces and proposed reductions
  - The strategy to accommodate parking demand is to maintain an effective amount of parking where most needed (i.e. business district) and consolidate where it is being underutilized
  - A large shift in parking to side streets is not expected
- More information needed on the relationship of bike lanes with slowing down traffic speeds
- Corridor-specific results from the CAC mobility survey were expanded upon by Andrew Overbeck of the CAC planning and development committee:
  - Respondents acknowledged the gap in the bikeways system
  - Discomfort when biking on the corridor in its current state
  - Difficulty crossing Indianola Avenue

Concerns around pedestrian and bicyclist safety were addressed and outlined within the project scope, including the addition of several new crosswalks, shortening the crossing distance with bump-outs and reallocating traffic lanes to reduce crossing distance and provide additional facilities. The technical analysis conducted thus far by Michael Baker International and reviewed by the City of Columbus and ODOT District 6 will be assessed in concert with input from the advisory committee members and the general public to inform the recommended design for the project.



## Walk Audit

The goal of the Walk Audit was to share community survey results with the public, and give an opportunity for the community members to learn more about the high-level aspects of the project and provide feedback in an open and engaging manner. Members who were unable to attend, had the opportunity to submit feedback, thoughts and concerns vs. email. Due to Covid-19 restrictions loosening, we were able to partner with Studio 35 and The Crest on this endeavor.

### Observations:

- Speeding along the corridor
- Light traffic volumes during non-peak hours
- Pedestrians frequently crossing near Glen Echo bridge
- Sidewalks too narrow with utility poles, trash cans, and other encroachments; too uneven to use a stroller or wheelchair
- Crossings unsafe for pedestrians at Hudson & Glen Echo bridge
- General safety concerns at the Hudson St. intersection
- Many pedestrians walked in the roadway; most cyclists biked in the roadway

### Interest in:


- Traffic calming and crossing improvements
- Bike lanes connecting Summit/Hudson to North Broadway
- Travel lane reduction
- Improvements at Hudson intersection and Arcadia intersection
- Medians and pedestrian refuge areas
- Safety improvements for pedestrian and bicyclists

### Emailed Public Comments:

- The rate of speed on Indianola outside my front door is excessively fast. Delivery trucks, utility trucks, and semis are the worse offenders.  
I'm at E Kelso and Indianola. Arcadia light to the light at Weber is a motor speed way.
- I'm not entirely sure what I'm supposed to comment on but I really think Indianola could benefit from calming, as could Hudson, especially between I71 and Indianola. The crossings at Hudson and 4th, Summit, and Indianola are dangerous for pedestrians. Indianola and Arcadia despite having a light also requires extra caution to cross. Indianola at the bridge over the Ravine is very dangerous to cross especially since it is a basin of sorts with cars speeding over the crests of the hills to the north and south. Visibility north is better than to the south for pedestrians crossing. There are stairs into the Glen Echo park at the north-east end of the bridge. It might be safer to install a second set of stairs on the west side of the bridge rather than a crosswalk across Indianola. I believe the majority of people crossing at this location are heading to or from the park. Alternatively a planted median across the bridge would help provide a safe spot to cross in two smaller crossings. Currently there is no center turning lane but four lanes of traffic across the bridge.
- Please be aware that the traffic at 1pm is nowhere near as heavy or fast as during morning and evening rush hours. I support calming / crossing improvements to this stretch of Indianola.
- Indianola between Weber and Hudson is busiest from 6-9 am and 4-7pm. It is often used by motorists looking to avoid traffic on I71 during peak times. That section of Indianola (even further north to North Broadway) is predominantly residential and should not be designed as a secondary commuting route to/from downtown. I believe that between Weber and Hudson should reduce to 1 lane in each direction with consideration for on street parking and dedicated bike lanes connecting summit/hudson to the bike lanes north of North Broadway. Currently during hours when parking is prohibited and traffic is two lanes, vehicles are traveling at high rates of speed just a few inches from residents on the sidewalk. I have seen and experienced too many close calls. The current sidewalks are not wide enough for two people traveling in opposite directions to pass one another.  
Replicating the median that was installed a few years ago in front of Studio 35 would also be an improvement. At a minimum something like this should be installed at Crestview-Indianola and Kelso-Indianola intersections to facilitate people crossing to the Crest Gastropub. There is also a lot of pedestrian traffic crossing near the Glen Echo bridge. A cross walk near the bridge or a staircase to the ravine on the west side of the bridge would reduce the risk to pedestrians.
- We live just off Indianola north of Arcadia and traffic flies by here at all times of the day/night. Please consider traffic calming. There are young children that live on Indianola and in the Glen Echo and Clintonville areas. Hudson should also be considered.
- As a Glen Echo resident and runner, I experience the sidewalks pretty intimately. While I may appreciate the off road feeling if the cracked and uneven walkways, a stroller or wheelchair would find it impossible to pass. There are several areas which the sidewalk is too narrow for a pedestrian to pass, as well. The pathways are also unpassable for strollers and wheelchairs because of this, too. Hudson and Indianola's northeast corner is of the highest priority. The turn from Hudson westbound to Indianola northbound has limited visibility and drivers often roll through, putting pedestrians in incredible danger. Earlier in 2021, a vehicle traveling west smashed into the northwest Indianola apartments. Sidewalks ending north of Cooke and in front of the Marzetti factory impede safe travel and force pedestrians onto the road. A friend was hospitalized in a coma for several weeks after being hit by a car while going southbound on

Indianola near Morse road. Key areas including over Glen Echo Ravine park could use garbage cans.

- I live a few blocks away from Summit St. And Hudson St. My family is young and growing (My husband and I are in our late 20s with an infant). Our area is increasingly becoming filled with people our age who want to walk to nearby restaurants, shops, etc. However I have lived in the area for 10 years now, within about 1 mile of this intersection. I would love the bike path at Hudson St and Neil to somehow be connected to the bike lanes at summit/4th for safer pedestrian/bike walking east-west. It would also be helpful to have Hudson St between 71 and Indianola changed in some manner to make it safer. Right now it's dirty, unsafe for pedestrians to be nearby, and is honestly even a bit scary when driving too. If there's a way this road stretch could have reduced speed, better traffic flow, and safer pedestrian walkways, that would be ideal!!
- I am writing to provide comments for the above-captioned survey. I am a homeowner on Summit Street north of the Hudson/Summit intersection that is part of the survey. The Hudson/Summit intersection is very dangerous when crossing Hudson by car, bike or on foot. The traffic on Hudson is very heavy and frequently exceeds posted speed limits. At least once a week I witness Hudson Street car traffic running the red light through driver error or with intent. I consider Glen Echo and SoHud to be walkable neighborhoods and believe that the present traffic situation is seriously detrimental to the general safety of the neighborhood and financial well-being of the businesses that are important to the community and rely upon those foot/bike patrons. Please seriously consider any and all changes necessary to address this issue. Allowing street parking and reducing speed to 25 would help. If Rt 23 needs to be rerouted to do that it should be considered. The primacy of the passenger automobile is out of place on this neighborhood street. Please help.
- I just saw this link and that there is something being planned for my immediate neighborhood. I very much hope that repaving the street is high on the priority list, especially between Hudson and E.N. Broadway. Otherwise, encouraging business to the northern area (south of Morse Rd.).
- I am a resident and homeowner on Summit Street, north of Hudson. I am writing to express excitement and support for complete streets planning on Indianola. First, I would like to offer some testimony on how my household currently travels on Indianola between Hudson and Como. I hope you will consider this as you plan the corridor.  
 WALKING/BIKING - CURRENT My husband and I walk along Indianola nearly once a week from Arcadia to the business district near Milford Ave. We tend to detour into side streets if the sidewalk is too crowded (trash cans, other pedestrians) or motor vehicle traffic is especially busy/loud (hard to hear a conversation, feels unsafe, makes our dog nervous). We walk multiple times a week with our dog along Indianola between Hudson and the Glen Echo Ravine bridge. We often pick up trash in this area as we walk. We cross Indianola as pedestrians at Arcadia almost daily during some seasons. My husband runs and bikes ~weekly/monthly along Indianola past Como Ave for fitness and occasional errands. Even when he does not travel along Indianola, he crosses Indianola on a daily or weekly basis.  
 WALKING/BIKING – FUTURE? I would love to feel safer and have more space as a pedestrian on Indianola. I would love to have the sidewalks cleared in the winter so it feels safe to walk. The slip/slide risk on icy/snowy sidewalks that are close to motor vehicle traffic discourages me from walking on Indianola in the winter. I would love to safely bike on Indianola so I can reach the business around Indianola and Broadway. I have always been too afraid to bike on Indianola due



to the lack of bike facilities (and it is too far to walk). For context, I have ridden my bike for commutes along Lane, Summit, Neil, and 4th. Indianola is too nerve-racking for me. I would love to be able to roller skate on Indianola sidewalks. I would love to worry less about my husband when I know he is running or biking on Indianola.

MOTOR VEHICLE – CURRENT Pre-pandemic I used CABS on Summit weekly and occasionally used COTA routes 4 and 31. I didn't typically catch the 4 on Indianola. However, I would like the COTA buses to operate predictably along that corridor since it feeds into Hudson and Summit. We have a personal vehicle that we use to drive north on Indianola about once per week. We rarely park on this street unless we are picking up dry cleaning. We prefer to walk to the businesses.

MOTOR VEHICLE – FUTURE? We like having the ability to travel northbound and southbound on Indianola with our personal vehicle. We rely on Arcadia as an entrance to the Glen Echo neighborhood (since Summit is one-way exit). As motorists, we are comfortable with reducing the number of lanes of traffic, introducing clearer pedestrian crosswalks, and/or adding traffic calming measures.

## Man On The Street Activation

The project team along with volunteers from the advisory committee polled passersby in the business district along Indianola Avenue roughly between Weber Rd. and Milford Ave. on Friday, July 9<sup>th</sup> from 6:00 p.m. - 7:00 p.m. and Saturday, July 10<sup>th</sup> from 12:00 p.m. to 1:00 p.m. Individuals and groups of people were interviewed, so there is not an exact number for those surveyed. A conservative estimate is more than 100 people interviewed over the two-day time period. Survey questions were structured to determine how people were traveling, where they were parking, and if there was interest in additional transportation options.

### Comments/Feedback:

- Most people drove to get there, while a quarter walked, and a few biked
- If driving, the majority parked on Indianola Ave, followed by Melrose Ave and Milford Ave
- When asked what would make it easier to walk, bike, or take the bus here, the most common response was bike lanes, followed by more reliable/faster bus service
- Patrons said when visiting they typically drove, walked, or biked

### In-Person Polling Results Summary

Approximately 70% of individuals and groups interviewed drove, while roughly 25% walked, 3% biked, and 1.5% arrived by another mode. When asked how someone typically traveled to the same destination, the majority drove, followed by those who walked. However, more individuals typically biked than what was observed on the polling days (Figure 3).

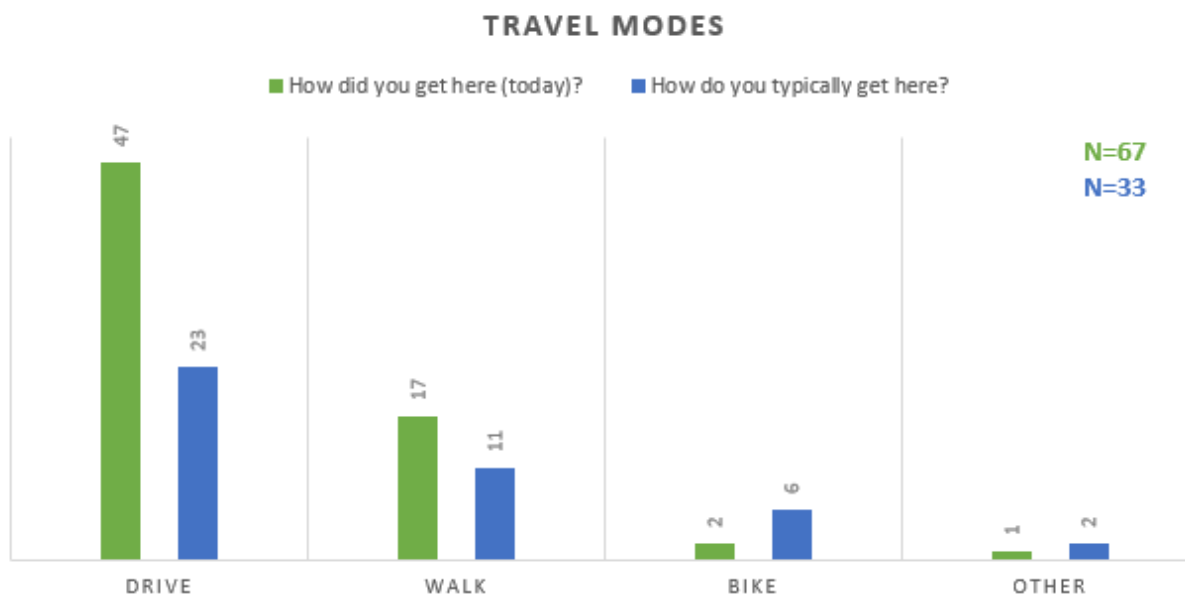


Figure 3. In-Person Polling: Travel Mode Questions

Those that drove were asked where they parked, with about 70% parking on Indianola Avenue, followed by side streets including Melrose Avenue (16%), Milford (7%), and other streets (7%) (Figure 4).

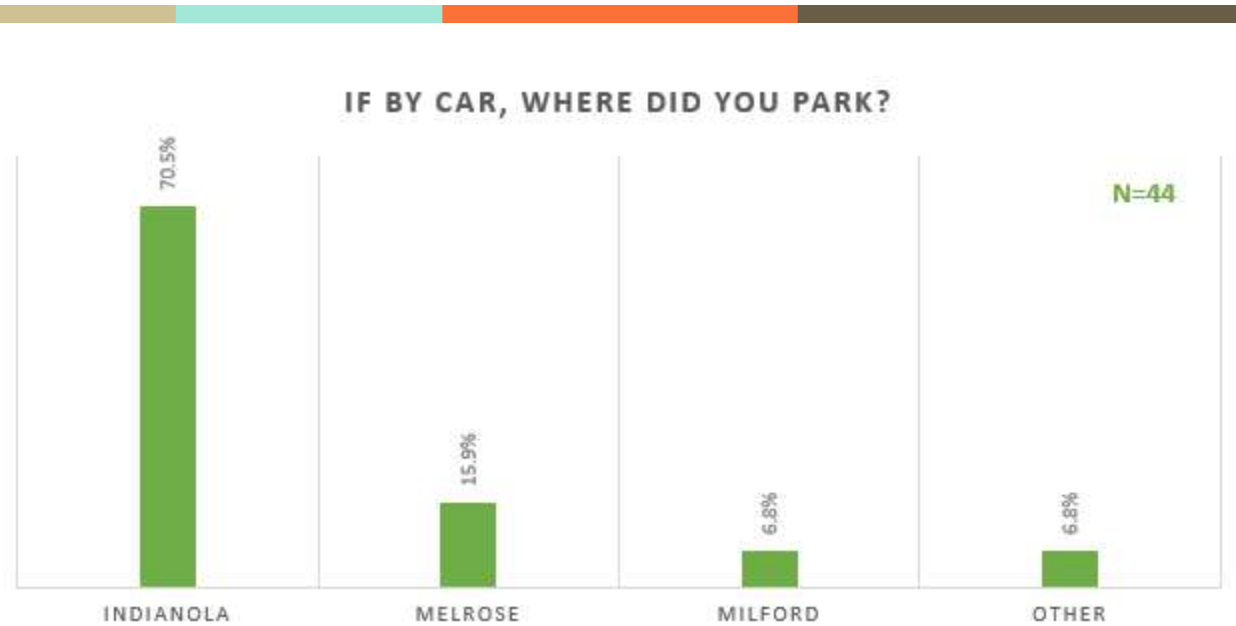


Figure 4. In-Person Polling: Parking

Participants were asked what improvements would make it easier to walk, bike, or take the bus to the business district area. \*Some surveyors asked the question more generally regarding what improvements would make it easier to travel on Indianola Avenue. Bike lanes were recommended most frequently, followed by more reliable and faster bus service, and updated sidewalks (Figure 5).

**WHAT WOULD MAKE IT EASIER TO WALK, BIKE, OR TAKE THE BUS HERE?**

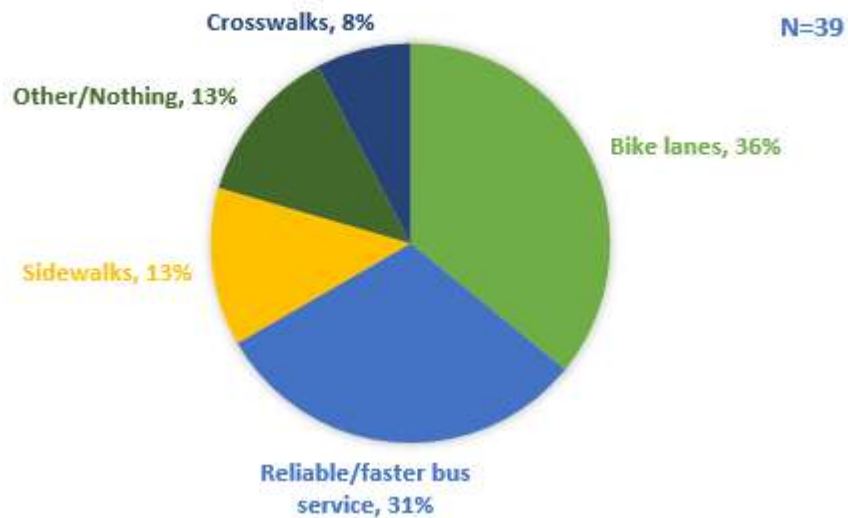


Figure 5. In-Person Polling: What would make it easier to travel on Indianola Avenue?\*



### HOW FREQUENTLY DO YOU VISIT THIS AREA?

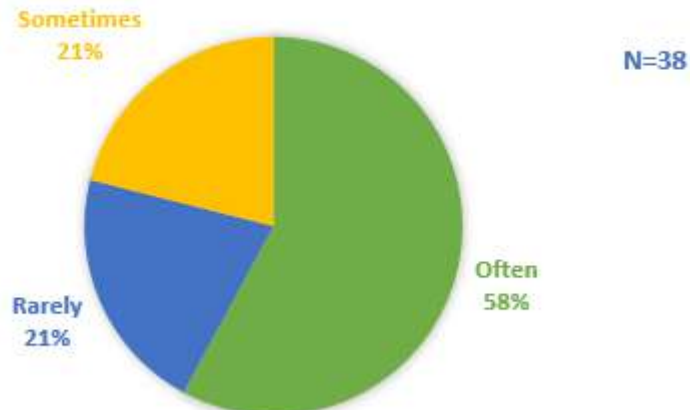


Figure 6. In-Person Polling: Frequency of Visits

To get a better sense of whether people were visiting from inside or outside of the neighborhood, participants were asked how often they visited, with many living nearby and walking to their destinations often. An even number was split between sometimes visiting and rarely visiting/first time visitors (Figure 6).

#### In-Person Polling Question Results

Specific responses to questions are outlined below.

Q1) How did you get here?	
Bike	2
Drove	47
Other: Dropped off	1
Walked	17
<b>Grand Total</b>	<b>67</b>

Q2) If by car, where did you park?	
Indianola	30
Indianola (south of Weber)	1
Leaned on street sign; unlocked	1
Melrose	7
Milford	3
Side street	3
Indianola	31
Side Street	13
Biked	1
<b>Grand Total</b>	<b>90</b>

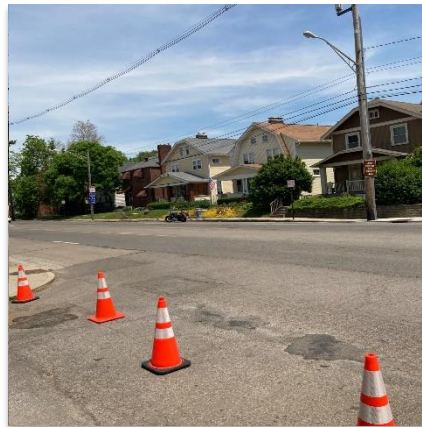
<b>Q3a) What would make it easier to walk, bike, or take the bus?</b>
Reliable bus service
More time (walk)
Would take the bus
Unsure
More information on how to take the bus; know more about existing bike infrastructure
Bike lanes
More information on how to take the bus
Faster bus service
More direct bus route
More pleasant walking experience
Bike paths
In better shape (to walk/bike)
Nothing
Pretty easy already
Faster bus service
Better sidewalks
Bike paths; walking paths
Live too far away to walk or bike
Crosswalks
Bike lanes
Bike lanes; better neighborhood sidewalks
Wider sidewalks
Crosswalks
More bus stops
Dedicated bike lanes (not at expense of parking)
Better traffic flow to work better with bikes; more crosswalks
Covered bus stops
Bike lanes
Better kept
Wider sidewalks
Bike lanes
Better transit
Protected bike lanes
Buffered bike lanes; bike racks; frequent & reliable bus route
Would take the bus or bike
Bike lanes; parking
Nothing
Bike lanes (similar to 4th and Summit)

<b>Q3b) Categories</b>	
Bike lanes	14
Bus Improvements	3
Bus Info	2
Crosswalks	3
Interest in bus	2
Nothing	3
Parking	1
Reliable/faster bus service	5
Retail	1
Sidewalks	5
<b>Grand Total</b>	<b>39</b>

<b>Q4a) How frequently do you visit this area?</b>	
Often	22
Rarely	8
Sometimes	8
<b>Grand Total</b>	<b>38</b>

<b>Q4b) How do you typically get here?</b>	
Bike	2
Bike, drive	1
Drive	18
Drive; bike	1
Walk	6
Walk, bike, drive	1
Walk, bike, scooter	1
Walk, bus	1
Walk, drive	2
<b>Grand Total</b>	<b>33</b>

## In-Person Engagement Photos





## Public Meeting

Two public meetings were held virtually on Thursday, September 16, 2021 at 12:00 p.m. and 6:00 p.m. Due to increased COVID positivity rates, the public meeting was moved from an in-person event to a virtual event. The meeting was advertised in the Clintonville Spotlight, through Studio 35, and online via Facebook. More than 100 people registered for the online meetings, with approximately 80 attendees across the two meetings, excluding the project team.

Warhol & Wall Street hosted the meeting via Zoom, with City of Columbus staff presenting on the project purpose and background, public engagement, study findings, conceptual alternatives, and next steps. Comments were recorded through the chat box feature in the application and through a verbal question and answer period at the end of the presentation.



Presentation slides, comments, meeting recordings, and the materials referenced in the study findings were posted to the project website following the meetings. A public comment period was open from Friday, September 17, 2021 to Wednesday, October 6, 2021 to submit comments online or through the project email. (Additional comments submitted past October 6<sup>th</sup> were accepted.) Public meetings comments and comments recorded during the public comment are included in Appendix A. The public meeting presentation slides are included within Appendix B.

Specific comments were received from the University Area Commission requesting safety and traffic calming improvements along the Hudson Street corridor. See Appendix C for the letter addressed to the Mayor's office, City Council, and the Division of Traffic Management.

186 comments were received during the comment period, with approximately 75% of comments showing support for bike lanes, traffic calming, or protected bike infrastructure. About 22% of comments were against the proposed recommendation, citing parking removal concerns. The remaining 3% of comments were neutral or unrelated to the alternatives presented. Out of all the comments, approximately half showed support specifically for protected bike infrastructure.

Public comments had similar themes, including:

- Need for protected and dedicated bike facilities with parking removal
- Improved safety for bicyclists and pedestrians in the corridor
- Designing for complete streets instead of car-centric design; considering future impacts and planning for sustainable and equitable modes of transportation
- Concerns over loss of parking for businesses, residents, and visitors of residents
- Need for wider sidewalks in better condition

- Desire for better bike facilities & support for removing more parking to support better bike facilities; perception that bike lanes are not being used on northern portion of Indianola
- General support for lane reconfiguration and traffic calming
- Safety concerns for Hudson/Summit intersection; requests for additional study of Hudson Street and connecting to North Linden
- Concerns over possible tree removal
- Questions on the validity of the study due to the impacts of COVID-19 on travel patterns

Selected comments (full comments available in Appendix A):

#### **Lane Reconfiguration/Traffic Calming**

- I am in 100% agreement that this stretch of Indianola should be two lanes of traffic with a middle turn lane. As a pedestrian on this street, I see that as a huge upgrade.
- I support calming / crossing improvements to this stretch of Indianola.
- Please consider traffic calming. There are young children that live on Indianola and in the Glen Echo and Clintonville areas.
- I believe that between Weber and Hudson should reduce to 1 lane in each direction with consideration for on street parking and dedicated bike lanes connecting summit/hudson to the bike lanes north of North Broadway.
- I personally commend the City for considering a road diet on Indianola. If we really do grow by a million people by 2050, Indianola is going to be one of the most important corridors in the City and it will need to become less auto oriented, more transit friendly, more bike and ped friendly, and safer for kids trying to get to and from school.

#### **Safety**

- A bike lane would not only give bikers like myself room to safely travel (which should be a right in contemporary cities), but also create a cushion for pedestrians who are constantly forced to walk in the street.
- Without protected bike lanes people are at serious risk in this area and many people who may otherwise be able to commute by bike won't do it if there is a section of their commute that is not safe.
- Alt 3 provides a safe option for all.
- Bike infrastructure is needed for this city to grow. This will allow more people to use the existing protected bike lanes on summit to get downtown efficiently and safely. Please adopt option 4 and remove street parking so that cyclists can move about safely.
- As a cyclist, I love to see any improved safety options to encourage bicycling and reduce auto traffic.
- Alternative 3 would provide needed traffic calming, while also building SAFE buffered bike lanes. The unbuffered bike lanes as proposed in alternative 4 are dangerous to cyclists and force motorists into making passes without giving the legally required space of 3 feet.
- Please select Alternative 3. If we are serious about Vision Zero in this city, that's the best option.



- There are options which increase safety for pedestrians and bicyclists, and if "Vision Zero" is actually the goal, then protected bike lanes are the only option that moves us toward the goal. When people feel it is safe enough to ride bikes, they will ride bikes more often.

### **Parking**

- There is rarely close on street parking by my house in the evenings. When my friends visit they have to park way down on Tibet. Any reduction of parking will only create further problems.
- Parking on Indianola, in front of our residence, is our only option for guests and utility trucks (east side of Indianola in the first block north of Arcadia).
- I agree that preserving as much parking as possible for the businesses in that stretch is important.
- I have ZERO concerns about the small amount of parking that is being reallocated to the bike lanes in this area. There is an over abundance of on-street parking in this location.
- There is zero downside to removing parking on this [Weber to Midgard] section in lieu bike lanes as there is an abundance of on-street parking options on the blocks adjacent from this location, especially the side streets.
- There is frequent overflow from the shops and the theater as it is, but it is manageable. Removing parking will drive more vehicles onto the residential streets, leaving residents with no place to park. This would of course make way for additional permit parking only zones, like we see on Crestview. I loathe the idea of becoming a permit only zone.
- As a former bicyclist, I appreciate the effort of creating a safer lane for bicycles to travel through. Unfortunately, this particular stretch of retail is not suited towards accommodating that AND having safe, available parking for our customers and for the customers of our neighboring shops.
- Please do not remove parking in this area. I live near Arcadia & the additional of 2 bike lanes has drastically reduced available parking.
- Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access.
- The city does not need more parking in Clintonville, and we already do not do enough for cyclists.
- Don't prioritize parking over active transportation. People on bicycles will bring more business to shops along the road more than a couple parking spaces.
- Favoring the convenience of parking over the safety from death or serious injury of a bicyclist would be an egregious oversight and an insult to the bicyclists of Columbus.

### **Sidewalks**

- We need new sidewalks that are wider and that don't have major obstructions right in the middle of the sidewalk. Even with the proposed changes, pedestrians will far outnumber bikers on Indianola, but the design decisions seem to be more focused on enhancing biking and parking at the expense of pedestrians.
- The beautiful sandstone curbs are in poor condition as are many of the sidewalks. Utility poles block the sidewalk so that strollers or wheelchairs cannot pass.

- The current sidewalks are not wide enough for two people traveling in opposite directions to pass one another.
- There are several areas where the sidewalk is too narrow for a pedestrian to pass, as well. The pathways are also unpassable for strollers and wheelchairs because of this, too.
- The sidewalks and curbs need to be fixed immediately, and the utility poles and signs need to be moved out of the pedestrian right of way.

### **Bikeways**

- Please include a dedicated bike lane.
- The added bike lanes on Indianola north of Oakland park are not usable. They are full of rocks and debris that make it unsafe for bicyclists to use.
- I see very few people using the bike lanes north of N. Broadway.
- Can the planter in the middle of Indianola near Melrose/Milford be removed to make way for the bike path? It would be safer than sharrow paint marks.
- I would prefer continuous bike lanes between Weber and Milford over sharrows, but I understand there are constraints and parking concerns.
- Connecting to the existing bike lanes both at the north-end and south-end of this project will fill a critical missing link in our greater cycle network.
- It is important to note that this plan reproduces structural racism, by not addressing the most obvious problem with access: the neighborhood of Linden. There is an opportunity to address this issue of connecting neighborhoods to the East to the rest of the bikeways of Columbus. Why does this study ignore Hudson Street, the most critical connector to the Linden Neighborhood?
- The cycle lanes that were put in from Oakland park to Morse enable me to bike to work in the Polaris area. Without them I would likely not do so. The current gap in bike facilities definitely needs to be addressed and I would welcome any improvements to increase safety and accommodations for non-motorists.
- Implement the change to allow for the safety of bikers and pedestrians along the public right of way along Indianola.
- I live in Glen Echo and support increased bike infrastructure, including bike lanes, as part of the Indianola improvements. Protected bike lanes would be ideal.
- Bike lanes should be a higher priority, extending the bike lanes further South on Indianola would benefit many individuals. It would provide a safer method of travel and promote a method of transport that has a much smaller environmental impact than motor vehicles.
- I would love more bike lanes in general, Indianola is a great place to start.
- A bike lane would not only give bikers like myself room to safely travel (which should be a right in contemporary cities), but also create a cushion for pedestrians who are constantly forced to walk in the street.
- I use the present bike lanes for errands and longer destination exercise. I like the sense of safety and separation from cars and trucks. I hope they continue to exist and/or improve.

### **Protected Bike Facilities**

- Transition from a dedicated bike lane to sharrow creates an uncomfortable and unsafe situation for cyclists which no amount of pavement marking or signage can change. There is zero

downside to removing parking on this section in lieu bike lanes as there is an abundance of on-street parking options on the blocks adjacent from this location, especially the side streets.

- Parking should be removed from this corridor and replaced with additional cycle infrastructure and a buffer zone.
- Ensure new 'protected' bike lanes are added - consider years from now how central Ohio and Columbus will be growing.
- While I appreciate that Alternative 4 is an improvement overall, I'm also concerned about forcing bicyclists to go from a dedicated lane to a sharrow.
- I would like to see Alternative 3. The city does not need more parking in Clintonville, and we already do not do enough for cyclists.
- Separated bike lanes and creating infrastructure for pedestrians and cyclists is essential for this road.
- Ultimately I hope to see buffered bike lanes all over the city. We shouldn't feel like we are risking our lives to commute by bicycle year round.
- Please do everything you can to include as many miles of protected or buffered bike lanes as you can! Protected bike lanes when connected to a wider bike system are proven to increase bicycle ridership.
- Adding cross walks and protected bike lanes is absolutely essential. Without protected bike lanes people are at serious risk in this area and many people who may otherwise be able to commute by bike won't do it if there is a section of their commute that is not safe.
- Actual barriers are much more effective at creating safety for cyclists than painted lines.

### **Sustainability**

- We can no longer design for "now" and our current car-centric behavior, but we need to design for a future that is sustainable and that is attainable.
- A car centric design is not a complete street!
- Bike and pedestrian deaths have been increasing nationwide while climate change continues to escalate. Improving indianola avenue will help people and the planet.
- Columbus needs to step into the future of car-free mobility options.
- We know that the majority of parking spaces are empty the majority of the time. We know that the parking lot surfaces contribute to heating up our city, increases the effects of climate change, lead to water runoff contamination, and contribute to flooding. Where we have existing roadway, it should be used in a way that will best for the community. Reducing the amount of parking we rely on is important.
- Reducing single occupant vehicular travel reduces carbon emissions and improves air quality, reduces parking costs and demand near downtown, and increases transit ridership. The benefits of increasing transit use are many but specifically increasing ridership increases COTA's operational budget to improve frequency and types of services offered, while reducing traffic congestion by decreasing the number of low-occupant vehicles on the road.
- having more bike/pedestrian friendly roads is a huge first step in cutting carbon emissions and make the city more inclusive over all.
- Given the threat of climate change and need to reduce car usage, I'd support seeing additional funding and attention put towards these projects in the future.



### **Hudson/Summit Intersection**

- Diverting the bicycle traffic through Glen Echo along Arcadia seems a reasonable response to safety concerns between 4th and summit, however it does not address the dangerous intersections along this corridor, especially at 4th, Summit, and Indianola and further East, especially at the Hudson/71 exits.
- The Hudson/Summit intersection is very dangerous when crossing Hudson by car, bike or on foot.
- We need to create some traffic calming on Hudson.

## Appendix A. Public Comments

A transcript of the comments recorded during the meeting and public comments submitted during the comment period are provided below.

### Public Meeting Comments

#### 12:00p.m. Virtual Meeting (Zoom)

##### **Poll Question: What is your relationship to Indianola Avenue?**

- Hi! I'm Molly (she/her). I live on Glen Echo Dr, parallel to Indianola!
- Transit Service Operations
- I live nearby. Between High and Indianola.
- Live nearby - Glen Echo. Walk it frequently.
- I'm a nearby neighbor, walker, biker, driver.
- Live a couple blocks away and bike on it regularly
- Michael - near life long resident of Clintonville
- I live on Milford, a few houses down from Indianola.
- Nearby (Hudson/Findley)
- Commuter along the corridor, patron of local businesses
- I'm a Columbus resident that loves to frequent the great businesses along Indianola. I'm also a bike commuter without a car.
- I am resident and live nearby and myself and my family often walk up to the businesses around Studio 35 and Savor Growl
- Clintonville resident and Planning & Development committee member.
- Live within 100 feet, bike, use transit and walk along Indianola and visit businesses along the corridor.
- Clintonville resident. Live two blocks away
- I live pretty close, it's my bus line and my main way in and around the neighborhood. I'm also the CAC Chair.
- Recently invested in 3017, 3021 Indianola Building
- Business Owner CPMM Services Group 3785 Indianola Ave
- live in the neighborhood (Glen Echo) walk/drive/bike but often avoid Indianola for all but driving because it doesn't feel safe

##### **Chat Comments**

- Love the idea for one at Cliffside! [*in response to crosswalk recommendations*]
- EB is extended bike lane?
- are these just car crashes?
- Interesting data to see the results of the road diet on Indianola. Love that it has been measured as proof of concept!
- Is it possible to remove the median if necessary to make room for bike lanes?
- would speed limit be reduced to 25mph in business district where sharrows are being proposed?
- Are the sidewalks widened in this plan?
- Very excited for the left turn onto Arcadia

- agreed, it will help with safety!
- would 4way red lights be a possibility at Arcadia and Indianola with bikes and pedestrians getting free both direction crossing while all cars are stopped?
- how will bike lanes transition to sharrows?
- how will buses interact with the bike lanes in this proposal?
- we have added office space which will add up to 17 individuals parking on Indianola during the hours of 9AM to 5PM
- This does not include the potential retail restaurant space planned for 3021.
- Can confirm - I feel safer on my motorcycle. and am not comfortable on a bicycle many places here.
- can you go over what happens on the bridge over Glen Echo ravine where the road narrows? would the center lane go away to allow the bicycle lanes to continue? how wide are the lanes? the ones already on Indianola are not safe to ride with kids or a trailer.
- I recognize this is a balancing act to serve all users and uses along Indianola. The addition of crosswalks and the consistent three lane section with bike infrastructure will improve this portion of Indianola for all.
- Agreed!
- If we have dedicated parking, can we bump out paving for traffic calming and pedestrian crossing?
- Hudson St is still the biggest challenge and the missing link to making this more accessible/equitable. As this group notes that it warrants more study, how can this be pushed forward now?
- in places with bike lane and parking, is it possible to have bike lane next to sidewalk so the parking is a buffer to the road?
- Are there limitations to bump outs because of "snow emergency" route restrictions?
- Same width on Milford from Indianola to the RR track (quick ortho measure). Where there isn't parking allowed on Milford, there are a lot of driveways and associated curb cuts. Seems worth taking a look at.
- I'd imagine daytime parking would be improved with the removal of the 7-9am and 4-6pm restrictions, correct?
- You could look at offering incentives to your office employees to use transit, walking and biking so they don't have as much need for parking.
- For business owners, it might be worth looking at the parking lot on the Christ International Church 382 WEBER RD to see if their lot could be used when they are not fully utilizing it. This could act as a satellite lot with proper signage and communication to patrons.
- I wish there was serious looking at making this corridor more bike and pedestrian friendly. It functions as a dangerous parkway to us bikers and walkers.



### **6:00p.m. Virtual Meeting (Zoom)**

#### **Poll Question: What is your relationship to Indianola Avenue?**

- I own and live in a house directly on Indianola.
- Resident
- I live on Deming and Cliffside
- I've lived on Indianola Ave for 19 years.
- business owner
- It runs through my CAC district.
- I live on the corner or ENB and Indianola. I am commissioner of CAC District 3
- home owner directly on Indianola
- I live nearby
- live on Indianola, since 2005
- Commute along the corridor (multiple modes) and patron of businesses
- Business owner, The Little Light Collective
- Business Owner
- resident of Milford Ave. right off Indianola, for 25 years.
- Homeowner
- Have lived on Melrose for 17yrs
- We live on Milford Avenue
- University Area Commissioner. Runner, bike rider, Glen Echo resident, customer of wonderful businesses
- cyclist and parent of young child
- Glen Echo homeowner
- Bike rider and transit rider
- Part of my neighborhood, I support the small businesses in that corridor.

#### **Chat Comments**

- I'd be interested in knowing the typical mode of transit in winter and inclement weather when visiting the business district.
- What's the parking like on the side streets during these times?
- What about flashing lights on demand at crosswalks like by the school on weber?
- anybody crossing at cliffside has to pray
- The parking study numbers look to be done during peak COVID shutdown. Is there an adjustment as our small businesses reopen?
- Parking on the side streets if not ideal in the evenings. I am at Indianola and Tibet and there are very few spots by my house after 6-7 PM
- the sidestreet parking should be taken into more account
- Could someone begin a campaign to educate drivers about pedestrians and that they need to stop for pedestrians?
- I agree, the study is worthless due to Covid impacts.
- Would the businesses be losing parking or losing convenient parking? Could the inconvenient auto parking be offset or improved by bicycle parking/visitors?
- they will lose parking. once side streets have issues the people on side streets will be asking for permit parking.

- I have questions about if putting the bus stop at Weber is the best idea? Wouldn't it cause congestion at the intersection?
- Are the number of total crashes noted on the slide, between the time period that was noted and for that particular section of street, particularly high? Low?
- I believe the number was 9
- I've noticed quite a few traffic accidents at Indianola and weber in the last few months. Seems like more than the last 15 years I've lived here
- Will more information be shared about the capacity analysis?
- the bike lanes north of n Broadway make sense since there isn't any street parking there. however, the bike lanes south of Hudson are tough to navigate and worry me as a driver and pedestrian there due to the street parking.
- The lockdown effected traffic flow and accidents.
- and capacity analysis needs to include reduction in travel due to covid.
- this works fine on summit and third with parking and bike lane
- maybe parking on one side only
- Agree that parking study and pedestrian counts are compromised. Webernola residential parking is already difficult when good movies or World Cup Soccer is on the marquee. Also can't get a wheelchair down the Indianola sidewalk so the parking lane is used
- I cannot imagine parking being removed and having people navigate trying to park everywhere else in the neighborhood.
- they could bike and walk more
- or ride bus
- Taking the bus also helps :)
- bikes using Calumet would also help.
- The parking restriction between the bridge and Hudson is useless, people only ever drive in one lane there except to either pass a left turner, or go 50mph, and it means nobody can ever park there unless they always leave their house from 7a-7p
- As a parent of young children, one at Indianola, it is not practical or safe to bike or walk the whole family down Indianola year round.
- That parking is not useless. I live on Indianola. This is where visitors park. It is also where patrons of the crest and Studio 35 park.
- 14 stops from my house to work. Never taking bus.
- There is a ton of side street parking
- I mean the parking from Hudson to Arcadia only, the rush hour restriction from 7-9a and 5-7p
- The bike lanes are an amenity that requires some compromise. The idea is more people will walk and bike with the infrastructure. Please consider this option when you're considering support or opposition.
- This is a State Route, it will always be a busy road. It will never be ideal for a nice family stroll.
- but it should be with all of the residential
- There will not be a ton of side street parking after the street residents begin to request permits.
- parking on side streets already difficult. And the beer trucks park in the suicide lane. Will they park in the fire lane instead?
- It can be family friendly. Other cities have made busier streets family friendly.

- As a resident of this neighborhood, biker and customer of many businesses between 2991 and 3055 Indianola, curious when those who live within the neighborhood will be approached for their thoughts, reactions and preferences on this project?
- car drivers don't know what sharrows are
- I am in this area and I was not approached for feedback.
- I don't think there is anyway to prevent parking in the neighborhood by non-residents. Addressing that is beyond the scope of this. I walk and ride the bus along Indianola and it is reasonably safe. Improvement in sidewalks would help. If the weather is truly unsafe to walk in, it's really not likely to be that much safer to drive in.
- We live in this area, why hasn't anyone asked us what we think? We live on Indianola
- there is a way to prevent non residents from parking. The area around the crest has permit parking. This idea will grow and there will be no parking for these businesses.
- Bike lanes generate new cyclists. Studies show that cyclists spend more money than drivers because they stop more often at businesses.
- the intersection with Duncan has the same problem
- If we build the lanes and work with the businesses, we can make the lanes work for everyone. We aren't impacting parking and not proposing a solution. Neighborhoods as far south as 5th Avenue will have direct access by bike to the business district. North Clintonville and Beechwood will have direct bicycle access to the business district. There is a lot of potential to increase customer capacity in the corridor.
- you are impacting parking. like I said this will create permit parking.
- I'm not clear on the transition for bikes going south. How do they transition to Fourth?
- have noticed having multiple lanes and parking restricted at peak leads to people trying to pass one another, or being surprised by a sporadic parked cars. have seen some cars struck from the rear because the restricted parking based on time of day leads to unexpected parked cars.
- I have also noticed some side streets like Milford have parking restricted on a portion of a block of the street, where parking is permitted on similar streets. is there an opportunity to add parking back in on some side streets?
- i don't see a lot of bikers using the lanes north of Broadway. Would like to see how many users a day took up lanes for traffic.
- if a cyclist takes the entire lane in a sharrow in this one lane section, they will be wrongfully blamed for the slowdown
- I echo previous comment about Calumet as a bike route. I always detour to Calumet on bike myself to avoid Indianola traffic, and it will still feel safer to me despite a potential indianola bike lane because there will still be less traffic there.
- removing parking restrictions is the dumbest idea yet
- Strongly recommend consistent infrastructure throughout the corridor. Sharrows aren't considered bicycle infrastructure by a lot of experts.
- do tax paying residents of this area get a vote?
- Can this be stopped?
- If that happens and comes that big of an issue, maybe the business owners can get together and develop a designated parking area nearby they maintain. Even when the bike lanes are empty, it creates a safer experience for pedestrians since there is more space between sidewalk and the cars.
- question: what is the final completion date? how long would this take to complete?

- Just want to point out that this corridor serves more people than just the local residents.
- I live right on Indianola and I've seen stuff about this for months on a lot of different platforms
- any residents on advisory or just businesses?
- Please send Libby Wetherholt an email with your contact information so that I can add you to my email listserv
- who is the advisory committee?
- advisory=businesses
- [libbywD3@gmail.com](mailto:libbywD3@gmail.com)
- For those of you against this please email me at [reedr137@hotmail.com](mailto:reedr137@hotmail.com).
- is there any evidence that sharrows work?
- there are not that many doors on Indianola to reach out to.
- Nobody gets door to door outreach on any project anywhere.
- we pay the taxes that build these roads
- there will be more permits.
- CAC sends postcards re: development projects before Zoning & Variance meetings to neighbors. Could that model not be replicated?
- unfortunately slowing down Indianola has caused the side street speeds to increase and cause unsafe conditions pedestrians
- Agreed. Thank you Eric for letting us all know that this zoom meeting was happening, as both a business owner on Indianola and a resident on Crestview I'm surprised I didn't find out about this any other way. I would've liked to have been surveyed.
- The fact that this plan has made it this far is awful. No community input until after they have a recommendation.
- hand out pedometers and make it a walking competition!
- There will be side street impacts from permit areas being requested.
- No one would want door to door.. how much do you with when someone knocks unexpectedly?
- I am really excited for this and can't wait to review the deck. Great work on really highlighting some great benefits of change! Thank you for being considerate asking us and taking time on this study work!
- Is it possible to only have parking on one side of the street?
- I really love the effort of this project and I have
- heard about it in a multitude of ways. Further, I run Indianola nearly daily and have to be constantly vigilant of cars turning into me. The bike lanes would provide an alternative to the sidewalks, which are very uneven and blocked on both sides (but mostly the east) by signs. This project would make me feel much safer personally. Similarly, I have had a friend hospitalized in a coma while riding because they were hit by a car. Specific area of interest for me is the Hudson Indianola intersection. I have nearly been hit by cars several times and the width of the sidewalk (which is not in scope, I believe) is too narrow for two people to pass at the same time on the NE corner
- Friends were not visiting during COVID-19
- please don't assume results of studies.
- The sample referenced 10pm on a Wednesday but weekends when businesses are having more events would be a better sample
- oh stop these are our houses. we pay the taxes.

- Absolutely
- The streets are public right-of-way and must serve everyone using the streets, not just resident parking. You live in an urban neighborhood.
- living on Melrose, it's been a huge pain already having people using parking...so it sounds like we need permits ...
- maybe we need a parking garage
- I am also very excited about improving safety for folks (like myself) who want to walk/ bike/ roll along Indianola.
- there will be permits everywhere.
- Would love to see Summit, Indianola, and Arcadia part of a more cohesive and continuous network.
- We will never be Chicago...
- Columbus isn't Amsterdam.
- oh, those scooters
- I have a preference for the green paint strategy and separate infrastructure for bikes. Shared lane strategies with sharrows have less of a safety benefit.
- sharrows don't work for single lane traffic
- Agree with Amber - the City should push for better and more consistent connectivity for the bike facilities.
- Amsterdam was once car dominant until it wasn't. It takes political will to get to that point.
- car drivers will verbally assault cyclists
- Great to see a commissioner here, thank you Katie!
- Also for everyone who is concerned about not having heard about this, I encourage you to provide that feedback to your area commissioner and make sure to engage with them. I got multiple emails about this from my commissioner
- I received multiple emails about this from Libby.
- Can more detail be provided about the supposed "capacity constraints" ?
- Shouldn't safety be a higher concern than moving cars through?
- I would love the bike path on Indianola! I don't think Calumet would be a good alternative because there are too many stops, specifically stop signs on that road to get moving and to have it be comfortable as a travel route. The 7 mile, mostly straight route is highly preferred!
- I personally commend the City for considering a road diet on Indianola. If we really do grow by a million people by 2050, Indianola is going to be one of the most important corridors in the City and it will need to become less auto oriented, more transit friendly, more bike and ped friendly, and safer for kids trying to get to and from school.
- People need to understand that not everyone is physically fit to ride a bicycle, can walk to a bus stop from their homes, can walk 6-10 blocks to a movie theatre or to visit other businesses in this corridor or other business corridors. You are not going to force people to use public transit nor is always practical. We need to look at all age groups of people and what their circumstances and situations are in terms of traveling. There needs to be some compromise.
- Is it true that this level of engagement for the corridor is unique for a resurfacing project? Thank you for having an opportunity for public input. It seems like most resurfacing uses the public right of way as it serves the general public best (not just immediate residents like myself). Roadway infrastructure is a public asset.

- another note, Indianola has been on the bike plan for quite some time as a future bike connection, and this section is the only gap between morse and downtown. It makes sense to connect morse road to downtown, and reduce speeds via a road diet. It does not make sense to maintain a gap of bike infrastructure. Bike lanes would also provide some buffer to the sidewalk and pedestrians instead of cars rushing by the narrow sidewalks, so it's not just for cyclists. it slows speeds, too.
- When are we getting continuous sidewalks on Indianola from Cooke to Morse?
- the crest already has to rely on valet parking on busy nights. this will impact business
- good question, I have to run in the bike lanes north of north Broadway due to lack of sidewalks
- Safety should be the number one concern, over trying to make these old neighborhoods and well traveled main streets viable for all avenues of transportation. Bicycles are a significant factor for maybe half the year in Ohio.
- Perhaps another way to increase parking would be improving the alleys behind businesses. The pot holes create huge alley-wide ponds. Even after the city "repairs" them, they fall apart rapidly.
- speed reduction will greatly help! agree chris
- Part of CAC's UIRF requests has been a section of sidewalk north of Cooke and south of Morse. Not sure when it might happen.
- Lower the speed and keep the current lanes.
- Is there any ADA street parking near the business districts? Would love to see some spaces converted if there is major concern about accessibility of persons unable to walk or bus or bike.
- public transit is already affordable, making it faster, more reliable, and more frequent and making sure that development decisions actually consider people's abilities to get to and from jobs and other amenities would go a long way to reduce our reliance on the automobile. It hasn't been that way for 100 years, but it was that way before hand. we're in the middle of an experiment here in America that is unique to this country and the automobile
- To Lark Witt - Submit a 311 about alleys and get your neighbors to do the same
- the current lanes cause issues with people attempting to pass one another. it's unsafe
- not very many sounds like a subjective term
- they don't repave alleys
- Can you make the renderings publicly available?
- again, those against this, please email me at [reedr137@hotmail.com](mailto:reedr137@hotmail.com)
- Love the call out for ADA parking from Dr. McNair
- Alleys are repaved
- To the comment about the crest and parking, they also give a discount to people who ride their bike to the restaurant
- Please prioritize safety for people who don't drive.
- its the "Super top secret" part that has been happening for two years I have an issue with.
- CAC District 3 Commissioner, Libby Wetherholt, [libbywD3@gmail.com](mailto:libbywD3@gmail.com)
- what is the timeline?



## Public Comments

The following public comments were received after the public meetings held on September 16, 2021 through the public comment period ending on October 6, 2021. Comments were submitted online via the project website or through the project email. Comments are organized by date received, with the earliest received listed first. Names and addresses were removed from comments.

1. Thanks for studying this corridor! I look forward to improvements that make it easier to bike and walk in the area. I would prefer continuous bike lanes between Weber and Milford over sharrows, but I understand there are constraints and parking concerns. I agree that preserving as much parking as possible for the businesses in that stretch is important. People often park in front of my house on Milford to go to Studio 35. Is there potential to open up some of the WB side of Milford between Indianola and Summit for onstreet parking as well? Also, the median at Indianola/Milford is a big improvement for pedestrian safety across Indianola over the previous condition, but I would hate to see that prevent the city from exploring all possible options for complete streets improvements along the Indianola business district. The need that led to initially installing the pedestrian median would likely be greatly reduced or altogether eliminated by improving walking/biking conditions along the whole study corridor.
2. I have concerns regarding the possible bike lanes on Clintonville. I live on Indianola and Tibet. There is rarely close on street parking by my house in the evenings. When my friends visit they have to park way down on Tibet. Any reduction of parking will only create further problems. Reduction of parking will lead to permit areas. Permit areas will negatively impact businesses. Calumet is a better alternative for bike traffic. Speed limits could be reduced to 25 on Indianola if safety is the main concern. I see very few people using the bike lanes north of N. Broadway. Any study or information gathered in 2020 to present is not accurate. You're analysis does not include Covid impacts. (This is a pretty embarrassing oversight). What can be done to stop this.
3. Thank you for last weeks information session. This stretch of Indianola has been in long need of multimodal enhancement. As nearly life-long resident of Clintonville I am hopeful this project carries forward. Not only will this provide critical connectivity into the cities bike-network, the road diet will provide much needed traffic calming along this stretch. Additionally, I have ZERO concerns about the small amount of parking that is being reallocated to the bike lanes in this area. There is an over abundance of on-street parking in this location. I'd rather see space reallocated to allow for more mobility options than continue to have we the taxpayers subsidizing on-street storage for peoples personal vehicles.
4. Since this plan has many negative impacts for those on Indianola avenue what is the plan to get this information spread to those who live on Indianola? The sole reliance on one business owner to get the word out will not suffice. The lack of a decent traffic pattern and parking study has me extremely concerned about this. Also, neighbors on side streets should be given information on how to turn their streets into permit only.
5. As a multimodal commuter, I appreciate the changes to make Indianola street safer for all. Still, it is important to note that this plan reproduces structural racism, by not addressing the most obvious problem with access: the neighborhood of Linden. Looking at the 2 slides titled "Bikeways" clearly shows that there is an opportunity to address this issue of connecting neighborhoods to the East to the rest of the bikeways of Columbus. The red line that indicates the "Study Area" is the problem. Why does this study ignore Hudson Street, the most critical connector to the Linden Neighborhood? It is not just for bikes, but also a connector for walkers, including myself, who feel unsafe right next to speeding car traffic. I understand that this is a

tough nut; going over a freeway and under a railroad bridge on a busy street. However, this is the tough work that needs to be done to help repair the history of redlining and racism that is baked into our current infrastructure. Please, let's include Hudson Street. Do not ignore this critical area that most needs our attention and change.

6. Please include a dedicated bike lane.
7. I'm writing to provide public comments about the Indianola plans. I live at 470 Walhalla, just steps from Indianola. I have lived here for 5 years. Background: I use Indianola every day, multiple times per day. I drive on Indianola almost every time I need to drive to or from my house. I walk my dog along Indianola every day. I like to bike to the Olentangy trail but I've never once biked on Indianola because it's unsafe. I probably still wouldn't bike on it after these changes. I'm more interested in how it would calm traffic and improve the walkability of the neighborhood. Before Covid I worked downtown and I would sometimes use Indianola to get to and from downtown (via Summit and 4th). I frequently visit the shops along Indianola, like Studio 35. My Thoughts on the Presentation. Moving to two lanes of traffic: I am in 100% agreement that this stretch of Indianola should be two lanes of traffic with a middle turn lane. As a pedestrian on this street, I see that as a huge upgrade. It's scary walking on the sidewalk when COTA buses and traffic are flying right next to you at 40 mph. Even as a driver and work commuter, I saw no benefit from four lanes. Because of parking (and illegal parking during rush hour) Indianola effectively has only two lanes (one each way) of traffic anyway. The rush hour commute was never improved with timed parking restrictions because it only takes one parked car to block off the lane and there were always 3-4 cars illegally parked during rush hour. This far from downtown parking is not enforced as closely. In short, we lose nothing as commuters and drivers by going down to one lane each way and as pedestrians we gain a lot. Walking Indianola: The sidewalks and curbs are crumbling and uneven. They need to be replaced. There are street signs and large electrical poles placed right in the middle of the sidewalk, which is a huge barrier to walking. It's very unpleasant to walk from my house just to get to Studio 35, which is only a 4 minute walk. The sidewalks aren't wide enough. Overall I'm concerned that this plan won't do anything to make this neighborhood more walkable. We need new sidewalks that are wider and that don't have major obstructions right in the middle of the sidewalk. Even with the proposed changes, pedestrians will far outnumber bikers on Indianola, but the design decisions seem to be more focused on enhancing biking and parking at the expense of pedestrians. Changes to Broadway: I'm concerned about any changes along Broadway that would result in destroying the tree lawns in order to expand the road. Broadway is one of the most beautiful streets we have in the city. Cutting down the trees and removing the tree lawns would harm this neighborhood substantially. I ride my bike in Columbus but I think most in this neighborhood would agree that harming Broadway to make it slightly more bikeable would be a big mistake. As a biker I prefer to get to High St on less busy roads anyway, like Walhalla, Clinton Heights, and Oakland Park. Even with a bike lane, Broadway will be much more dangerous than those roads. I would hate to see Broadway ruined just to open up a bike lane. Overall I am very excited about the prospect of removing lanes of traffic on Indianola. I'm hoping that we can also use this project to make Indianola more walkable. I also hope that we don't do any harm to Broadway, which is a beautiful street and should stay that way for the next 100 years.
8. A couple of questions about the Indianola repaving project. Will the Complete Streets project include any work on the curbs and/or sidewalks? The beautiful sandstone curbs are in poor condition as are many of the sidewalks. Utility poles block the sidewalk so that strollers or wheelchairs cannot pass. Is there any plan to fix this? I like the idea of the bike path, but I

wonder how the ends of the bikepath will connect. Right now the bike paths along Summit and Indianola end abruptly leaving riders to figure it out. How would that part work? Will any additional trees be removed as part of this? We lost a few more big ones when they replaced utility poles. Can the planter in the middle of Indianola near Melrose/Milford be removed to make way for the bike path? It would be safer than sharrow paint marks.

9. Regarding the "Indianola Avenue Complete Street Study": I am writing to express my concern about the considerations underway that might take away parking for residents like myself who live on the east side of Indianola Avenue in the first block north of Arcadia Avenue. I live just south of the Indianola bridge over the Glen Echo Ravine, and in my block there is a ravine running behind each of the properties. It runs from the Indianola bridge all the way to Arcadia Avenue. As a result, there is no option for parking on a side street in the event that the area of Indianola in front of my residence becomes a 'bike path only' lane instead of continuing as a "restricted-hours parking" lane. I own and live in the duplex at 2700-02 Indianola, and we have exactly two off-street parking spots per unit. That means that if on-street parking is taken away in my block, anyone who comes to visit us (friends, relatives, plumbers, electricians, home-healthcare nurses, etc.) will have no reasonable place to park when they arrive at our residence. For most other Indianola properties guests can perhaps park on the nearest side street, but even then, where are they supposed to have the cable technician or the gutter cleaner to park when we need any type of home maintenance service. Some people might have an alley they can use for that purpose, but in our case, there is no alley running behind the house - only a ravine. Parking on Indianola, in front of our residence, is our only option for guests and utility trucks. My husband and I are in our late 70's and early 80's, and losing the ability to have our visitors or maintenance people park on Indianola Avenue (currently allowed anytime except 4-6pm weekdays) would create an extreme hardship for us and for our tenants. I also own the apartment building at 2626 Indianola, and since there are 4 apartments and 4 parking spots in a garage accessed from the alley behind the property, I frequently have to park on Indianola when I need to take care of something at that address. And again, maintenance people also share my need for on-street parking in front of that property.
10. The added bike lanes on Indianola north of Oakland park are not usable. They are full of rocks and debris that make it unsafe for bicyclists to use. The precious road diet was a waste of money. Don't make the same mistake south of Oakland park. I'm against further road diet on Indianola.
11. First, I would like to thank the project team for all their hard work thus far on the proposed project. As a 20+ year resident of Clintonville this section of Indianola has been in long need of improvement and the additional for more multi-modal infrastructure. Connecting to the existing bike lanes both at the north-end and south-end of this project will fill a critical missing link in our greater cycle network. However, I would like to address the plan for sharrows in the Melrose to Milford section as shown in Alt. 4. I would urge the project team to seriously reconsider the plan for this block section. Transition from a dedicated bike lane to sharrow creates an uncomfortable and unsafe situation for cyclists which no amount of pavement marking or signage can change. There is zero downside to removing parking on this section in lieu bike lanes as there is an abundance of on-street parking options on the blocks adjacent from this location, especially the side streets. Additionally, it is important for the project team to remember, the city conducted a road diet on W. Broad St in Franklinton which removed ALL of the parking on Broad. So I wonder why the City is caving to preserving parking here, white & wealthy

Clintonville after they stuck by removing parking from largely low-income & black/brown Franklinton?

12. I live off of Indianola just north of Morse Rd. I am very concerned about the speed limit here (45 MPH) being too high. There are many deer being hit by cars on this stretch of road and many people will go much faster than 45. It's a safety concern. Also, many people will walk alongside the road here because there is no sidewalk. With cars going that fast, I'm always very worried about peoples' safety. Thank you for taking this information under consideration.
13. 1) How does a traffic study conducted during a Pandemic reflect valid and applicable data for years beyond the immediate now? = to base improvement decisions on a study of little to no bearing in the future is worthless. 2) Current designs where motorists have to turn in front of bicyclists traveling right of them at Cook road for instance, endanger rather than safeguard both parties. 3) The sensible thing would be to move east and west bound traffic on Weber through a tunnel, similar to EN Broadway. 4) While Indianola may be a much travelled thorough fare, it is also a neighborhood street connecting many a business to its community members. Creating an additional bike lane here would take away from on-street parking and create a rather crammed street scape that endangers the many pedestrians on Indianola sidewalks in this area. 5) To improve the pedestrian experience, it is necessary to designate Clintonville as a no-idle zone as well as turning parts of it into a pedestrian zone. 6) Safe and reliable transit service, i.e. buses and bikes rarely mix! 7) Utterly underused is the alley running north south to the train tracks which would also allow to pull bike traffic off of 4th Street and Summit and provide for a safe and exclusive ride all the way to downtown on the east side, why the same has already been accomplished on the west side through the OSU wetlands. 8) Since more and more folks are switching to e-bikes, especially among the senior community, the question is, how will they be faring on a busy street like Indianola to age in place safely?
14. I am writing to express my support for the complete streets redesign on Indianola ave. I live within a block of Indianola and bike frequently on it. The cycle lanes that were put in from Oakland park to Morse enable me to bike to work in the Polaris area. Without them I would likely not do so. The current gap in bike facilities definitely needs addressed and I would welcome any improvements to increase safety and accommodations for non-motorists. Based on the presentation slides it seems only one option is being considered but generally it looks positive. I would encourage buffered cycle lanes wherever possible (not separated from car lanes by parked cars!!!) and sharrows only where the road is too narrow for dedicated lane. Slower traffic speeds and better sidewalks would also make it more likely for me to walk along this street. As it stands I don't find it safe with my young children. Parking is not a concern for me as I rarely see many spaces in use and the study seems to confirm that. Furthermore I don't think a street this busy is appropriate for parking; it only makes the area more dangerous. I also firmly believe it is not essential for businesses to prosper and have seen many studies to back this. Thank you for working improve this area and our neighborhood.
15. Implement the change to allow for the safety of bikers and pedestrians along the public right of way along Indianola. Also extend a safe way all the way to Lincoln along Indianola
16. I live off of Indianola on Midgard Rd. and many of the homes do not have usable garages, if any. My elderly neighbors already struggle with keeping the parking spots in front of their houses available. It's a quiet street with retirees and young children who currently are able to play safely in it. The bike lane would push traffic and noise into our already narrow street and negativity impact our peace and quality of life, and it will negatively impact our treasured small businesses on Indianola.

17. Please DO NOT take away the parking along Indianola between E.N. Broadway and E. Tulane. The businesses there need that space for parking. I live on Milford which "dead ends" at Studio 35 so we already have the limited spaces on our street being used, which, most of the time, we're do not mind. To take the Indianola parking away for bike lanes would be disastrous for our neighborhood. The bike lanes are not really needed in this area, the ones farther north on Indianola are very rarely used.
18. I am a 22 year resident of Tibet Rd near Indianola Ave. I recently learned of your proposal to add a bike lane between North Broadway and Hudson Ave. I'll be brief. If this involves reducing the number of parking spaces I disagree with this proposal. Having good businesses in the area between Walhalla Ave. and Arcadia Ave on Indianola Ave is important. They already have enough obstacles to overcome to succeed. Reducing parking spots would be another unnecessary one.
19. I do not have a driveway and rely on parking on my street! I am aware that the city is proposing to add a bike lane along Indianola between N Broadway and Hudson. I find this to be sadly detrimental to the small businesses along Indianola between Milford and Weber. I'm afraid if parking spots are removed, the businesses will suffer by folks not finding close parking (not to mention the struggle they are already facing due to COVID!) Personally, I am worried about not finding close and convenient parking on my street...(which is not ideal for a mom of two little children!) The few bikes that ride down Indianola have plenty of room to head north and south safely as cars can move to the middle to safely pass them. We are so proud of our neighborhood and the value that these wonderful businesses bring to our community. We want them to succeed. And we want our property values to continue to rise!
20. I am a home owner in the area being studied. I believe a bike lane along indianola would reduce the number of parking spaces along this route to an unmanageable number, pushing cars to park along the side streets that are already difficult to navigate. Increased traffic in this area would be dangerous to the many children who play in these neighborhoods. Additionally, parking when patronizing businesses along indianola is already bordering on untenable. A bike lane would only make that process largely untenable.
21. I am in full support of the removal of the recommended parking along this corridor. I am a driver as well as a cyclist living on indianola and Como. As someone who lives and bikes on this street, Im concerned about the safety of cyclist using this corridor. Removing parking and potentially reducing the width of the street will act as a damper on how fast people are able to go. As for the feared decrease in patronage from customers, it seems unlikely that someone would drive all the way to the theater and not stay because they have to walk one block further from a parking spot (which are abundant on all the side streets in this corridor). If business are concerned about patrons not knowing where to park they can erect signage about parking around the corner. Slowing the pace of traffic on the road and even creating a bump out for additional seating would do wonders for the street life along this special energetic corridor. It could be a place to come and stay not unlike central worthigton but it will require a redesign of the street.
22. This proposal addresses a serious pain point for cyclists in Columbus. I would estimate less than 50% of Indianola has parked cars, and so cars seem to travel significantly faster where there are two lanes available. This leaves no room for bikes, and I'm often passed at high speed with very little room to spare. And when there are parked cars, I need to get all the way in middle of lane (where cars close in at what seems like 45mph). A bike lane would make me way more comfortable when using this corridor, similar to the sections above Oakland and below Hudson.

And on that point, addressing Hudson is long overdue. Since the road joggles, and there are buildings up to corner, it's hard to see traffic coming and crossing is really dicey. Feel like I risk my life every time I pass through, though it's a necessary interchange to easily travel south (only alternative is to meander through narrow residential streets).

23. I am part of a small business on Indianola north of Weber. I urge you to please reconsider the bike lane on Indianola due to the limited amount of parking there is already on that street. I fear taking away more parking spots will deeply impact sales (negatively) for small businesses on that strip due to limited and less accessible parking.
24. I am writing in opposition to the proposition of a bike lane as currently prescribed. I have a small child and a reduction of the street parking is going to push cars into my neighborhood. Instead, rather, I think it's prudent to ask the question of how many bikers are using Indianola and what the use is. Is it mostly recreational or is it commuting? And when they are doing it. I say this as you could time-switch the activities along Indianola. For example, in the mornings M-F between 7-9am part of the street is now for bikers in order to accommodate commuters. On the weekend it could be from 7 to 10 am to accommodate recreational biking. All other times it's for cars and parking.
25. Indianola Ave from Borth Broadway to Hudson is perfect the way it is. Changes do not always need to be made, especially if something is great the way it is. Our neighborhood is a community and the construction and addition of bike paths is unnecessary and will effect this area in a more negative way than good. The businesses on this street are close to everyone's heart and we must look out for them. Reducing parking is a major concern. Along with the people of the neighborhood. A bike lane will cause an unintended consequence of reduced parking and push more cars into the side streets of residential areas. Everyone in this neighborhood is considerate and conscious allowing bikers to ride safe and happy. Don't mess with a good thing. Clintonville is perfect the way it is. Put government money to use somewhere else, where it is truly needed.
26. Yes, safety is of the utmost importance for pedestrians, bikes and cars. However, Indianola Ave. has already been cut down to one lane for the most part, especially along the two block business area. There is hardly any place to park now, and the overflow is on the side streets. With few driveways and garages, these side streets are already congested. The homeowners rely on this on-street parking. Eliminating approximately 139 parking spots on Indianola would be a disaster for everyone except the bicyclists. You must look at the frequency at how many bikes actually use Indianola on a daily basis, versus parking spots that would be lost. These small neighborhood businesses are struggling and this would definitely create even more hardship for the majority. I have lived in the Clintonville Community for over fifty years, and I have seen a lot of changes. However, I strongly oppose to this one. We do not need a bike lane between EN Broadway and Hudson St.
27. The current parking situation on the east and west sides of Indianola between Weber and Midgard should remain.
28. Thank your for considering needed improvements to the Indianola corridor that would not have ordinarily occurred with an ODOT repaving project. It is important to make the corridor safer for all users, and the complete streets improvements on Indianola north of North Broadway prove that these methods help to slow traffic and reduce accidents. I am confident that the proposed changes to Indianola south of North Broadway will have the same positive impact, while also providing needed safe crosswalks and bike lanes. While reduced, I believe the on-street parking provided will continue to adequately serve existing businesses and residents. I recognize that



some compromises have been made to serve all users, but I believe these improvements on the whole will make the corridor safer for all. My only other comment would be to include new curbs and sidewalks as part of this project as many of the curbs and sidewalks along Indianola are damaged and crumbling.

29. A few more thoughts about the Indianola paving project. What is the intended audience for the bike path? Is it for commuters traveling fast or recreational riders moving at a slower pace? What are the types of destinations for these bike riders? Will there be more bike path connections to make their rides safer? I see how the Summit St bike path ends at Hudson and shudder to think they would try to negotiate with speeding traffic and narrow lanes on Hudson. I am concerned about other bike path segments where the path ends in locations that may not be very safe for bicyclists or pedestrians like Indianola and Morse. The plan for the business area near Weber keeps the functionless planter in the middle of the road and mixes bikes and cars together. Get rid of the planter and there would be room for parking and a bike lane. Someone had suggested a two-way bike path like Summit St with parked cars providing a buffer between bikes and moving traffic. I think that feels safer to users, but does cause a visibility problem for cars entering/exiting Indianola from side streets. Right now, delivery trucks park in the suicide lane on Indianola to make their deliveries to Webernola businesses. Could there be a way to improve the alley behind those businesses to provide truck access for deliveries to the rear of businesses? Right now it is pretty overgrown with a sharp turn at the end that is pretty difficult for larger vehicles. During the last neighborhood parking battle, one solution was for businesses like the Crest and Studio 35 to provide valet parking. They had parking agreements with local schools that weren't using their lots at night. That arrangement didn't last long. Is that why the Crestview neighborhood had to resort to permit parking? How do those residents feel about permit parking now that it is in place? The Parking memo seems to show that there was only 1 data point for the Webernola business area. Was side-street parking considered in the parking memo? How were the peak use hours determined for the data point shown in the Parking Memo? Will the areas with the highest parking utilization have new parking restrictions. Side street parking is already pretty tight at peak business times. Is the project scope just paving and restriping, or will pedestrian and mass transit users see any improvements? It would be nice to have a sidewalk where 2 people can walk side by side. What are the plans to move COTA stops from their current locations? How will public comments be addressed? Will any questions be answered? Will there be another zoom call to hopefully hear about from neighbors or social media? Will any outreach to residents be initiated?
30. We need to create some traffic calming on Hudson. I wonder if it's been studied how a roundabout at the Hudson and Indianola intersection might help calm traffic? We need more green medians all along this route like the ones in front of Studio 35.
31. An extended bike lane on Indianola will cause traffic backups in both directions at N Broadway. It will reduce parking and push that into the neighborhood, already crowded with shoppers and customers. I propose taking the bike lane to west on N Broadway to Calumet, south to Hudson, east to Summit St / N 4th. Please do NOT extend the Indianola bike lane from N Broadway south.
32. I become livid when I, who am disabled, have to walk because Melrose is completely full. I have lived on Melrose for 22 years and the parking with Studio and street market events has become outrageous. All we need is to cut the parking on Indianola; as well as impacting tax revenue for the city with numerous businesses.

33. I can support the bike lane on Indianola north of North Broadway as it utilized a vehicle lane to implement the bike lane. I can also support the bike lane on Summit south of Hudson for the same reason. I cannot support the removal of parking to add a bike lane. The street parking on Indianola between North Broadway and Hudson is vital for both residential and commercial uses. Please reconsider other alternatives in that corridor!!
34. I am a homeowner, cyclist, and patron of the businesses along the Indianola corridor, so I have multiple interests in the infrastructure plan. Generally, I approve the thoughtful planning for this project. You've maintained the critical parking near local businesses, while improving pedestrian and bicycle infrastructure. I have two comments: 1) I question the validity of the parking survey on 9.26.20 at 7 pm as there were no parked cars at a time when Studio 35 should have been busy, except for the pandemic. However, the plan preserves parking in this area. 2) The proposal shows sidewalk improvements that are sorely needed in this corridor. In some cases, street signs are placed in the middle of the sidewalk. The plan doesn't address street trees. There are mature street trees in this corridor that should be protected and new trees should be added. I would also like to see street trees planted along Indianola north of North Broadway to improve the pedestrian and cycling experience and further slow traffic.
35. Cutting parking in half when there isn't much parking already would not be beneficial for the residents that are being challenged having a spot close to where they live. It would also decrease any revenue that the smaller business owners are struggling with currently. Please don't let that happen!
36. I am a resident of Glen Echo and frequent user (walking, biking, and driving) of the Indianola Complete Streets Corridor. I am encouraged that traffic will be calmed and bicycles will be **\*\*more\*\*** accommodated in this corridor. I support a reduction in travel lanes and a reduction in parking. I also support the additional crosswalks, especially the crosswalk at Cliffside where speed is usually an issue. I recognize the limitations of this particular iteration of this project (curb to curb), but would like the Committee to understand that this should be a first step and not the final solution for multimodal transportation along Indianola Ave. Widened sidewalks, planted medians, street tree containing bump outs, and protected bike lanes should all be part of the plan for the near future. I also want the Committee to consider agility in its plan should the UAC letter sent to the City with respect to the Hudson St. connection prove successful. Finally, in Alt.4, I'd like to see the parking lane closest to the driving lane vs. the bike lane. (Bike lane nearest sidewalk.) This would further constrict the ability to speed and provide some protection to bicyclists without losing any parking spaces.
37. We need the parking spots more than the bike lane. Having lived in the area for 25+ years I am well aware of the traffic and pedestrian flow. The bike traffic hasn't been very high on Indianola. The existing businesses and new ones to come need the parking spots.
38. I am writing to request that the bike lane project for Indianola Avenue be rejected. As a resident of Milford Avenue, I witness the traffic patterns here daily. Not just during the atypical pandemic interview. Unfortunately, these old neighborhood streets were not designed for the inclusion of additional modes of transport. There is quite a lot of traffic through here, congestion at times, and difficulty turning onto Indianola from side streets. Already, the parking is insufficient, at best, for the struggling businesses. Neighbors are unhappy with the amount of cars lining our side streets, since they are unable to park near their homes. Some have no driveways for parking. We do NOT want to require permits in this neighborhood. We DO want to encourage small businesses here. There simply is not room for a safe bike lane that would be used by a small number of cyclists during maybe 6 months of the year. The existing parking

spaces are needed. Sometimes projects that look good on paper are not practical. p.s. Glad to hear that repaving is on the agenda!

39. I live at the corner of Indianola and E Kelso. Pedestrian crossing at this intersection is dicey and a marked crosswalk would be helpful not just for neighbors but also restaurant patrons. Also turning left/south from E Kelso to Indianola is basically a blind turn when parking is full on the west side of Indianola. This is a daily occurrence especially during restaurant hours. Employees park there so it is constant not just when patrons are coming and going. A vehicle must pull out past the parked cars to check for oncoming traffic. Weekly near misses are the norm. This is probably all information you are already aware of just wanted to add my two cents.
40. As a resident of Melrose ave I am strongly against this proposal. It will give minimal benefit and will greatly harm the parking situation and safety of our neighborhood. We already have enough issues of people parking in the neighboring streets to Indianola, reducing the available spaces on the road itself will only exacerbate the problem further. It could potentially lead to permit only parking and that is the last thing I want for my neighborhood. In terms of safety, this will only encourage more people visiting businesses to park in neighborhoods adjacent and this could encourage more theft from cars. There was a recent event held behind our block of businesses and the excess parking from that showed exactly how dire the situation is in this neighborhood already. Cutting off what we have will only cause that nightmare to happen on a daily basis. I strongly urge you to not go forward with this proposal.
41. Hello, I'm in favor of the Complete Streets Initiative for the Indianola Avenue corridor. However, I don't want this to be a complete streets approach in name only. According to Smart Growth America, "Complete Streets are streets for everyone. They are designed and operated to prioritize safety, comfort, and access to destinations for all people who use the street, especially people who have experienced systemic underinvestment or whose needs have not been met through a traditional transportation approach, including older adults, people living with disabilities, people who cannot afford or do not have access to a car, and Black, Native, and Hispanic or Latino/a/x communities. Complete Streets make it easy to cross the street, walk to shops, jobs, and schools, bicycle to work, and move actively with assistive devices. They allow buses to run on time and make it safe for people to walk or move actively to and from train stations." (1) Prioritizing the needs of a few, especially with regards to parking does not seem to fit in with the complete streets ethos. The city of Columbus is working towards being more equitable and designing the streets with a true complete streets approach is the way to achieve that. In the slide deck from the public meeting on 9/16/2021, there's a diagram of what appears to be a bus queue jump. (2) I'm in favor of giving the bus access to signal priority and jumping ahead of the personal vehicles. This is an example of how we can improve our bus system and make transportation in Columbus more equitable. Thank you for your time. I'm looking forward to building safe, complete streets for everyone. (1) Smart Growth America <https://smartgrowthamerica.org/program/national-complete-streets-coalition/publications/what-are-complete-streets/> (2) NACTO <https://nacto.org/publication/transit-street-design-guide/intersections/intersection-design/queue-jump-lanes/>
42. I was unable to attend the meeting, but have reviewed the slides and have some input/comments. I have lived on Como Ave. just off of Indianola for over 10 yrs. As a resident, I have studied the Indianola corridor for many years, including some research projects from when I was getting my Masters degrees in city and regional planning and Landscape architecture. In particular I was looking at the slides that showed the existing conditions and alt design sections.

I noticed it said that "These images are for illustrative purposes only. Dimensions are preliminary and are under review" however, I feel like they are misleading in terms of representing the spatial volumes and the actual conditions that exist. In many areas along Indianola the sidewalk is bordered on one side by the street (as shown) but the other side is often bordered by retaining walls anywhere from 1' to 5' in height. If not retaining walls then steep slopes and/or steps, shrubs/trees/vegetation etc. These conditions are typically immediately adjacent to the sidewalk. In reality, it's a stark contrast to the "wide -open" and flat sidewalk space that has been inaccurately illustrated in your slides. It's actually quite restricting and narrow feeling, especially when you factor in the street signs, utility poles, refuse/recycling containers etc. that are often occupying some of that space, especially when you consider that there is no tree lawn to separate a pedestrian from the curb and traffic. It's difficult, sometimes impossible for 2 people to even walk side by side, let alone to allow someone to pass from the other direction. In most cases, one has to step into the street to avoid obstacles or other pedestrians, because there is no room on the other side due to the aforementioned retaining walls, etc. that border the sidewalk. None of the Existing street sections show this narrow condition or any of the obstacles (utility poles, signs etc.) However, almost all of the photos used in the slide show exhibit these obstacles and conditions. Slides 8, 14, 15, especially 18, 23 to a small degree and 26 again. None of the alt. show any options for dealing with these obstacles, widening the sidewalk and/or adding tree lawn buffers between the sidewalk and roadway. Pedestrians using the sidewalk will still be relegated to a very narrow 4' passage unless they want to risk stepping into the street, be it a drive lane or bike lane, still dangerous and unaccommodating to a pedestrian utilizing the sidewalk. It is not a safe, accommodating or comfortable condition along the sidewalks, and none of the Alternatives seem to address those issues at all. Is there any consideration being given to the "streetscape" character? It would be nice to see some options that address the street tree canopy. Over the years it has been butchered/decimated to accommodate old and new utility lines. Can we bury some of these utility lines? Do we need utility poles on BOTH sides of Indianola? A tree lawn, or intermittent bump outs would be great opportunities to add street trees along the corridor, in addition to potentially creating/adding strategies to address stormwater issues in the form of rain gardens etc. I would be happy to volunteer more input and potential solutions, and I'm sure many of my neighbors would too. It seems that the opportunity for community input and participation has been quite limited, and not very well publicized.

43. As owner of Elizabeth's Records, a retail vinyl record purveyor since 2010, I can't imagine our shop being what it is anywhere else. Our shop is at 3037 Indianola, in the strip of retail northwest of Weber. Our store is situated in the bus zone already, which is was a situation we accepted when we moved in. As a former bicyclist, I appreciate the effort of creating a safer lane for bicycles to travel through. Unfortunately, this particular stretch of retail is not suited towards accommodating that AND having safe, available parking for our customers and for the customers of our neighboring shops. As a card hanging disabled person, I need to have access to my vehicle nearby and not parked down a side street. Our property does not have rear parking or access to any entry from the back. While we considered the parking situation when opening our shop here, we never imagined the prospect of losing the limited street parking we already have. This shop is my family's livelihood, and as a disabled man of 60, I'm not sure where else we could afford to move and have similar success. This location is exactly where we wanted our shop to be and exactly where we hope to stay. Please consider the devastating effect taking

away our street parking would have upon us and on our fellow family owned shops around us! If you should seek further comment or information from us please call us at 614-569-6009.

44. Bike lanes on Indianola would be supremely problematic for our neighborhood - residents and the businesses. There is frequent overflow from the shops and the theater as it is, but it is manageable. Removing parking will drive more vehicles onto the residential streets, leaving residents with no place to park. This would of course make way for additional permit parking only zones, like we see on Crestview. I loathe the idea of becoming a permit only zone. Do not do this.
45. I am a 19-year long homeowner of the 3076 Indianola Ave address and will be affected by the changes that are made to Indianola Ave based on the Complete Streets Study. I wanted to offer my input. For starters, I support the addition of the bike lanes. I think the only way for this to work would be to remove the parking restrictions. In the area between Weber and Midgard (Area 4 on the map), I think you should mark the parking spaces. We are the only driveway in this area, and our driveway gets blocked constantly despite the fact that we have a fresh, new, glaringly obvious driveway:



If you create an X'd out block on the road before and after the driveway, like in front of 3021 N High St, I think it will greatly reduce people's desire to try to park there:



Another option would be to create something similar to the area in front of 306 E N Broadway with a tapered line and/or vertical markers:





We also like the option of creating a bump out to help calm traffic like at 3016 N High St. I would suggest this be more of a bike rack area than a bench area.



As far as adding crosswalks, I suggest that you also add the flashing crosswalk sign like those located to cross Weber to Indianola Alternative:



All of Indianola Ave is virtually unwalkable. The sidewalks are narrow and butt right up next to the road:





I think creating unrestricted street parking would help with the safety issue of cars speeding dangerously close to pedestrians. I think adding parking lines, again, would help accentuate that this area is not for driving. Another alternative that you might want to consider in making this area more walkable, is widening sidewalks or creating a hell strip next to the road to act as a buffer. There are a few schools in our area, and I fear for the kids that have to walk down that busy street. I have two kids myself that I do not let walk to school alone because of the fear of an accident. You really need that buffer there. One wrong step, and you could be a goner. An additional item that I ask you to consider is lowering the speed limit to 25, especially in the area between Weber and at least Walhalla where there are a number of businesses and a lot of foot traffic. Cars tend to speed through that area and it's very much a safety concern. Sometimes my children want to walk to Libby Lou's (the candy store) and I worry every time. Some people speed down Indianola as if it were 71. We need to do everything we can to get the traffic speed to a reasonable level. It's way worse than the study determined. I also ask you to consider using the road space that is just north of 3061 Indianola Ave. and the road space in front of Studio 35 for additional parking. I do not understand why these are marked as NO PARKING zones. If the space in front of Studio 35 is restricted due to a desire for a Loading Zone, I would like to point out that I never see that area used for that purpose. Instead, the beer trucks typically pull up into the middle turn lane to park and unload their deliveries there. So, to summarize, we REALLY need to slow down traffic, and I hope some of my suggestions will help with that. We need to make the area safer and more friendly to pedestrians. Also, since the road is being repaved anyhow, I would really like to see a solution to our blocked driveway situation. Please feel free to call and discuss further. Thank you for your time.

46. Please include a dedicated bicycle lane as wide as you can down Indianola Ave. Make sure that it is a protected lane so people will use this and stay safe. We want to make a low stress biking experience. Please put the parking in between the vehicle travel lane and the bike lane to make a parking protected bike lane where parking is needed. (this also prevents people from getting "doored")
47. I'm writing to support the City's efforts to improve Indianola Street's access and safety for all roadway users, including for people who walk and bike frequently along the corridor. Improving the street by allowing other modes of transport besides a car will bring Columbus closer to

parity along the lines of income and racial background. I support Alt 3 as it will best allow people of all abilities to move without a car in a safe manner and bring Columbus one step closer to being a leader in Bicycle mobility.


48. I often bike, drive, and park along the Indianola corridor. I would love to see a protected bike lane along this route. This would make me feel far more comfortable biking along this route with less confident riders as well as reduce the personal danger I experience. Frankly I'm not sure why any parking is needed on most of Indianola. I have never had any issues parking on the side streets (which have the added benefit of not being time restricted). I rarely encounter more than a handful of cars parked along Indianola itself except at times in certain locations (i.e. in front of the Cinema). Pretty much all of the houses along the corridor are serviced by an alley and I suspect nearly all of those houses have off-street parking off of their alley. I have also never experienced any sort of traffic on Indianola itself, and feel it could easily be reduced to a single lane in each direction. When coming from one of the bike lane options to the south (Summit or 4th), the Hudson intersection is the most dangerous. I typically skip this by riding through the Glen Echo neighborhood, but getting across Hudson can still be dicey due to the congested intersections with minimal bike infrastructure. I hope this feedback is helpful.
49. I would like to express a concern with Alternative 4. The Melrose to Milford segment which forces cyclists into a shared drive lane with a center median would be extremely dangerous in practice. I have found center medians like this with only a single driving lane cause drivers to get road rage and often try to pass cyclists without enough room. The current conditions are somewhat ok because the existing drive/park lane rarely has parked cars in it, and so I can simply use the curbside drive lane as a cyclist. If the parking restrictions are eliminated and this is turned into a dedicated parking lane, I am certain that this segment of road will actually be LESS friendly to me as a cyclist on this route. I would suggest either eliminating northbound parking under this alternative or eliminating the turning lane to make enough room for bike lanes. Alternatively, leave the existing setup. A single drive lane with sharrows is far worse than the current setup with a park/drive lane.
50. This project needs to also focus on Hudson Street from interstate 71 to High Street as well. Actually Hudson from Cleveland Avenue to High Street. Thanks for engaging the community.
51. We need buffered bike lanes with street parking removed. I frequently bike on Indianola and it is DANGEROUS. Why not expand options for people without cars and encourage more people to ride bikes? It's better for the health of commuters and residents.
52. As a resident of the city of Columbus, I support the city's effort's to make Indianola Ave safe for all the people who travel up and down Indianola—including cars, busses, bicyclists, and pedestrians. This is also an opportunity to shape the future of Columbus into a vibrant, healthy, and accessible city.
53. Our city needs to move into the future. A future that is safe for people and encourages transportation that decreases our carbon footprint. We will never achieve this if we do not act on our aspirations by changing our infrastructure in ways that changes human behavior. Please be courageous and install bikeways in our city that place a buffer between bikes and cars! Option 3 is the way forward to a safer and more sustainable future.
54. As resident of the City of Columbus, I'm writing to support the City's efforts to improve Indianola Avenues's access and safety for all roadway users, including for people who walk and bike frequently along the corridor. Improving the corridor for people who walk and bike will also improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small

business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrows (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its implementation. The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Continue the Kelso to Weber cross section through the Weber to Milford segment. In addition, transitioning from bike lanes to sharrows is a design appropriate for 1% of people and does not encourage new cyclists. Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.

55. I fully support the addition of bike lanes on Indianola Ave. I would like to see the bike lane and the auto lane divided by parked cars whenever possible. I think it is a great protection for cyclists. Thank you
56. We really need protected bike lanes and better protections for pedestrians on this route. Anything to slow cars and separate traffic will help. In a neighborhood with so many children, families, and schools, preventing gruesome death and injury should be the highest priority.
57. I am very supportive of the addition of bike lanes to Indianola Avenue. However, there are some changes that should be made to the project to make it safer for all users. Marked crosswalks should be provided at every intersection along the corridor to provide consistency throughout the corridor. Intermittent crosswalks confuse drivers and can make crossing at unmarked crosswalks more dangerous. Some of the proposed crossing locations are ~1000 ft apart. Pedestrians are not going to walk nearly a half mile or 10 minutes out of their way to cross the street. They are going to cross where it's most convenient and the street should be designed to make those crossings safe. Additionally, the bike lanes need to be carried consistently throughout the entire corridor. The bike lanes should not end for three blocks just to provide storage for personal vehicles. The addition of bike lanes will encourage more people to travel by bike to the businesses along Indianola (multiple studies have shown the addition of bike lanes are good for business); ending the bike lanes into sharrows will discourage those same people from traveling to this area by bike. This comic is a great example of what ending a bike lane into sharrows feels like for people riding bikes: <http://stevepatrickadams.com/if-roads-were-like->

[bike-lanes/](#). If the center median is the reason bike lanes cannot be provided in this section, then it should be removed and potentially reconstructed in an appropriate location.

58. As frequent cyclist through the City of Columbus, I'm writing to support the City's efforts to improve Indianola Avenue's access and safety for all roadway users, including for people who walk and bike frequently along the corridor. Improving the corridor for people who walk and bike will also improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrow (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its implementation. The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Continue the Kelso to Weber cross section through the Weber to Milford segment. In addition, transitioning from bike lanes to sharrow is a design appropriate for 1% of people and does not encourage new cyclists. Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.
59. As resident of the City of Columbus, I'm writing to support the City's efforts to improve Indianola Avenue's access and safety for all roadway users, including for people who walk and bike frequently along the corridor. Improving the corridor for people who walk and bike will also improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.
60. I support alternative three (3) for the Indianola Avenue Complete Streets Study.
61. We need to have better lighting at crosswalks to make pedestrians and cyclists more visible at night. As for the overall look, I favor Option 3.

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62. As resident of the City of Columbus, and a student at OSU, I'm always supportive of additional bike infrastructure. Biking is my main form of transit, and another corridor to safely navigate off-campus is deeply appreciated, as it makes my commutes safer and less stressful. Improving the corridor for people who walk and bike will also improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrows (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its implementation. The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Continue the Kelso to Weber cross section through the Weber to Milford segment. In addition, transitioning from bike lanes to sharrows is a design appropriate for 1% of people and does not encourage new cyclists. Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.
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My comments below address specific elements of the study and the proposed design (Alt 4).  
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66. Separated bike lanes and creating infrastructure for pedestrians and cyclists is essential for this road.
67. Please do not remove parking in this area. I live near Arcadia & the additional of 2 bike lanes has drastically reduced available parking. I have no idea why they felt the need to put a bike lane on each side of Arcadia, it is simply ridiculous.
68. After some very careful consideration I am unable at this time to support the bike lane proposed for Indianola as the preferred option by The City of Columbus due to the large reduction in available on street parking. As we have talked about in the public meetings, I don't believe that the parking studies that were completed during the height of COVID reflect the true nature of the parking use and needs on Indianola. In addition, the surveys within the community, it is my belief that if you ask anyone if we should install a bike lane, why would you say no? It sounds like the right thing to do and we all should be biking more. If the question was asked differently: Would you give up on-street parking for a bike lane?, I think we can assume the results would be dramatically different. Other questions and concerns: How many of the CAC survey respondents live on or around Indianola? I think that this survey could be flawed; it could be like asking someone in Worthington about a bike lane that is going to be installed in Bexley. How many people currently use the bike lane on Indianola? How many people are expected to use the bike lane on Indianola? If the road is shared with bikes instead of specific bike lane, would that change usage? Make it safer? Is there empirical evidence to support this conclusion? How do the bike lane usage numbers justify reduced parking? What is potential financial impact of business with reduced parking? Was this taken into consideration? What about the weather affecting bike lane usage? Time of year? Time of day? How are people going to cross Indianola to get to their destination safely, due to the reduction of parking? Unless the sidewalks and crossing options are improved (illuminated and marked), will it make the road more safe?

Only restriping is being considered. Should there be an uptick in accidents etc, would there every be an option to eliminate the bike lane? Has the City ever removed a bike lane? One of the objectives is to get traffic to slow down on Indianola, are there any studies or projections to show how this goal will be achieved? Business is tough enough coming out of COVID and I think anything that makes it harder (less parking) and pushes people to park farther away from our businesses is a terrible idea. It will also push parking onto the side streets, potentially angering the residents causing permit parking on the side streets. If this happens, it will be detrimental to business and an obstacle that couldn't be overcome. I believe it is reasonable to make as much parking available to our businesses because accessible and thriving businesses contribute to the tax base to continue growing our city.

69. Please make more bike lanes everywhere!

70. I support alt 3, buffered bike lanes.

71. This area is in significant need of updating - sidewalks are nearly unusable due to overhead wire poles. Cycling is dangerous. Parking is a concern. Many rely on the parking on-street as there are few alternatives nearby. Residents may reserve the side-streets for parking. Is there a location that could be repurposed for parking? Alley or empty space behind the businesses? All in all I support option 4, but the overhead lines could be buried or moved. Businesses should be consulted and aided in retaining accessible areas for customers to park. Residents must have parking so side-streets are not made permit-only during business hours.

72. The under 50 percent parking utilization figure is staggering. We are giving up safety of pedestrians and cyclists, many of whom are young children in an area we want to draw young professionals in order to provide car storage for, at max, 88 residents. Parking should be removed from this corridor and replaced with additional cycle infrastructure and a buffer zone.

73. Go with Alternative 3. Stop half-assing public transportation in this city for cars.

74. Yes, put in legit bike paths that are safe and do not have to compete with cars. Put them everywhere!! Especially crossing 71 on Hudson, North Broadway, Weber, & Cooke. It is impossible to get from one side of 71 to the other. People that live on the East side of 71 have a dangerous path to get to the bike lanes on 4th and Summit to go downtown. I would like to ride to work. Thanks :)

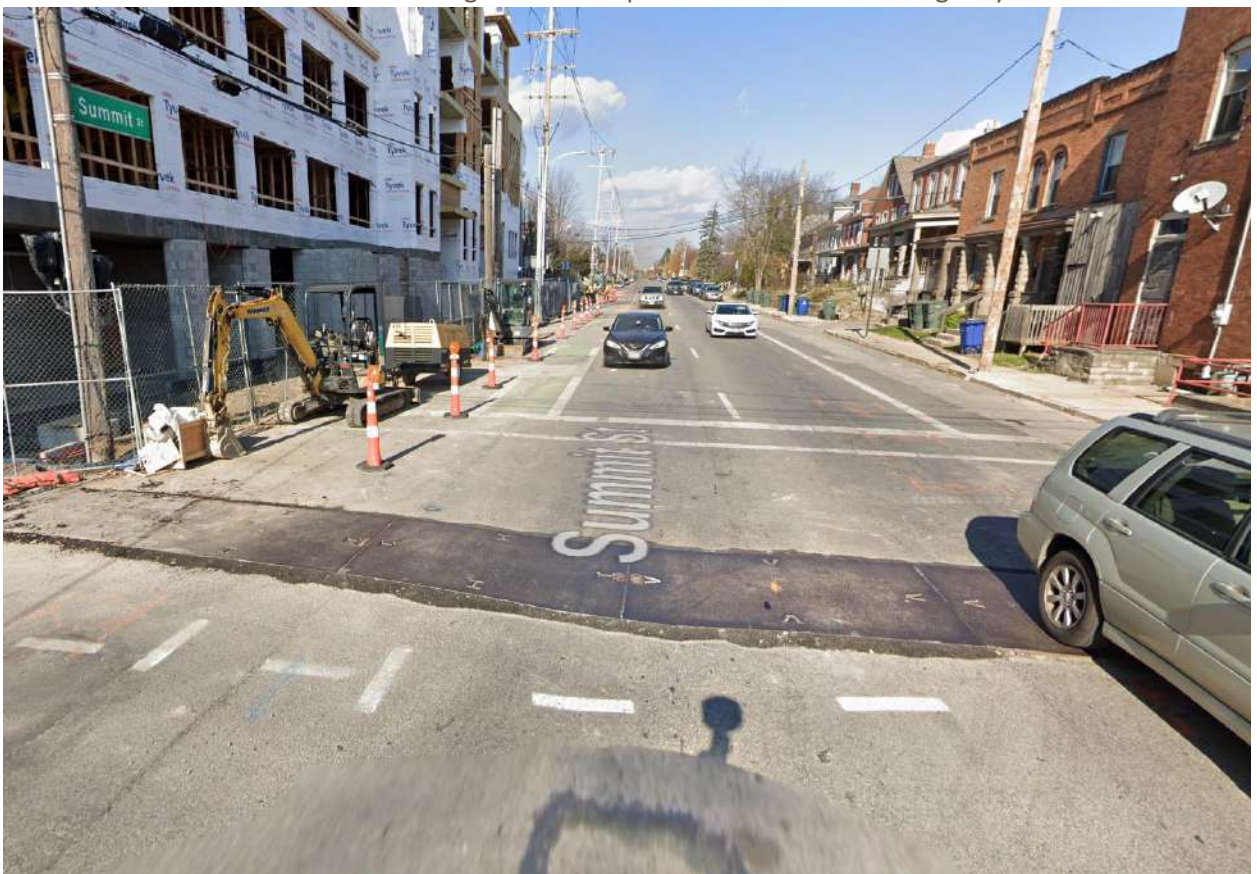
75. I like alternative 3. Buffered bike lanes are the safest and most pleasurable option for bike infrastructure. If we want pedestrians to bike more, we need to make it attractive.

76. As resident of the City of Columbus, who also frequently bikes down Indianola for most of my commute to work, I'm writing to support the City's efforts to improve Indianola Avenues's access and safety for all roadway users, including for people who walk and bike frequently along the corridor. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone (and, I'll admit, my safety specifically). I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at



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77. As a cyclist who often rides Indianola I am concerned about a proposal which has bike lanes throughout the majority of Indianola but switches to sharrows for a small section between Weber and Midgard. It can be difficult and dangerous to merge from a bike lane into a vehicle traffic lane. Cars do not tend to be looking out for cyclists doing this (as they are not presently in their lane of travel) and usually do not want to be slowed down by cyclists, which can cause them to try to accelerate past rather than allowing the cyclist in. This was a frequent issue for me at the corner of Summit & 5th during construction these past 2 summers. Construction blocked the bike lane, forcing cyclists to move into traffic moving at a much faster pace. If you were lucky, this would happen as the light was turning red. I've attached an image of the intersection from Street View. The image was taken prior to the bike lane being fully blocked.



The alternative to making unsafe lane changes then is to forego the bike lane prior to the sharrows. This of course also invites angry drivers who don't understand why a cyclist isn't using the bike lane. I'd much prefer to see parking removed between Weber & Midgard to allow for a continuation of the bike lane.

78. As someone who frequents businesses in this area, alternative 3 (remove parking, provide buffered bike lanes) would be ideal for me. Being able to safely access these areas without lugging a car around would be a great convenience, and it's clear that we as a city are paying to maintain far too many parking spaces here.
79. I understand I am using a partial form letter, but the points are succinct. I support this effort to making this stretch of Indianola and surrounding areas more bike and pedestrian friendly. Improving the corridor for people who walk and bike will also improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4).  
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80. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. Alt 4 provides a substantial improvement from the current design, but also has drawbacks that should be better addressed if Alt 4 is advanced. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The corridor already sees significant bicycle and pedestrian traffic. Similar projects suggest that more residents would bike through the corridor with improvements to level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the

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82. I am a big fan of the Alt 4 proposal for installing bike lanes on Indianola. I ride that route 3-4 times a week year round and having a continuous bike lane from campus area to north Clintonville would make riding safer and more comfortable. I have one concern about the proposal from Melrose to Milford. A shared lane there is a bottleneck and I have personally experienced drivers dangerously passing me to get in front before the median. I would advocate for continuing a separate bike lane through that area, even if it required removing parking just for the length of the median. That would only remove 5-10 spaces, and there are ample parking spaces on Indianola just north of that area.
83. As resident of the City of Columbus, I'm writing to support the City's efforts to improve Indianola Avenues's access and safety for all roadway users, including for people who walk and bike frequently along the corridor. Improving the corridor for people who walk and bike will also

improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrows (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its implementation. The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Continue the Kelso to Weber cross section through the Weber to Milford segment. In addition, transitioning from bike lanes to sharrows is a design appropriate for 1% of people and does not encourage new cyclists. Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.

84. With one exception, the dozen businesses on Indianola between Weber and Milford do NOT have dedicated parking lot/spaces. Their customers rely solely on street parking, and there is extremely limited parking available on side streets. Please consider keeping street parking along that segment of the corridor.
85. As resident of the City of Columbus, I'm writing to support the City's efforts to improve Indianola Street's access and safety for all roadway users, including for people who walk and bike frequently along the corridor. As a pedestrian, recreational cyclist, and bike commuter who lives in southern Clintonville near Indianola, I want to weigh in. Improving the corridor for people who bike and walk will improve the safety for all street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. I think Alt 3 provides the best opportunity to improve the corridor for everyone. Alt 4 provides a substantial improvement from the current design, but also has drawbacks that should be better addressed if Alt 4 is advanced. I feel like going for Alt 4 at this moment is an incomplete measure, a missed opportunity, and might lead to bigger costs eventually as the city and community realize that buffered bike lanes without street parking are a better option for a human-scaled, walkable neighborhood. My comments below address specific elements of the study and the proposed design (Alt 4). - Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly



raise the visibility of each crosswalk and establish consistency through the corridor. "Dutch-style" crosswalks in which the crosswalks and sidewalks are at the same height are safer and more accessible. - When the bike lanes drop at intersection approaches, install super sharrows (colored paint to mark the shared bicycle lane) and other treatments to increase the visibility of the infrastructure. A general sharrow is not sufficient to create a comfortable experience for the majority of people interested in riding their bike through the corridor. - The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Transitioning from bike lanes to sharrows is not an accommodating design for most of people and does not encourage new cyclists. Work with the community and business to develop a strategy to address any business owner concerns around parking without sacrificing bicycle accommodations. Again, business can increase by improving bicycle access. Sharrows have been shown to be unsafe for cyclists, pedestrians, and motorists. Buffered or segregated bike lanes should always be preferred, especially in wide streets such as Indianola. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.

86. I support Alternative 3 for buffered bike lanes on Indianola to connect the bike lanes north of Oakland Park to Arcadia and Summit/3rd. Safety should be prioritized over parking and driver convenience. Crosswalks should be painted at every intersection, including unsignalized ones.
87. I live in Glen Echo and support increased bike infrastructure, including bike lanes, as part of the Indianola improvements. Protected bike lanes would be ideal. Hudson Street should also be investigated for safety improvements to connect the corridors of 4th and Summit. Traffic calming or other measures should be implemented immediately due to extreme safety issues on Hudson.
88. Ensure new 'protected' bike lanes are added - consider years from now how central Ohio and Columbus will be growing. We may see the advent of commonplace driverless vehicles and we should ensure that we are well suited for this shift. Here's the facts: central Ohio won't likely see the investment of a large-scale public transit system beyond busses; with this in mind, the next big change for traffic in our area will be driverless vehicles. Given this information, we should endeavor to structure the city to handle this change and reduced need for on-street parking, so protected bike lanes and better protected pedestrian areas would seem the best path forward.
89. I am a resident of Columbus, and I want to offer my strong support for Alternative 3 - buffered bike lane with no parking. There is great need for better bike infrastructure and walkability in our city. While we can accommodate 500,000 - 1 million more people in Columbus, we cannot accommodate that many cars. We need to create alternatives to driving, With the rise of e-bikes, biking is becoming a suitable option for a much wider range of people, including older adults. But people won't ride unless they feel safe. Alternative 3 with a buffered bike lane is the safest alternative, and the most likely to get more people riding. I strongly urge that the city implement Alternative 3. Alternative 4 is OK, but not as safe and not as likely to get more people riding. Alternatives 1 and 2 are more of the same old car dependence. That is not working, and it will work even worse in the future.
90. Alternative 3!
91. I am resident along Indianola Ave at the intersection of Midgard and Indianola. I am firmly in support of traffic calming measures in this area, and I think Alternative 3 is the best solution for this street. Traffic calming is desperately needed on Indianola. As I live north of the medians at Weber & Indianola, I see northbound vehicles accelerating to dangerous speeds as they clear

the medians. The street is build like a freeway, and the 35 mph signs are meaningless to motorists. Alternative 3 would provide needed traffic calming, while also building SAFE buffered bike lanes. The unbuffered bike lanes as proposed in alternative 4 are dangerous to cyclists and force motorists into making passes without giving the legally required space of 3 feet. Cyclists must frequently navigate obstacles, debris, vehicles and pedestrians obstructing bike lanes. To do this safely, a buffer space between motor vehicle traffic and the bike lane is imperative. There is little demand for on-street parking along Indianola Ave, outside of the shops north of Weber. The street parking available on residential side streets can more than suffice for this demand. I strongly recommend alternative 3.

92. I would like to see Alternative 3. The city does not need more parking in Clintonville, and we already do not do enough for cyclists.
93. I'm writing to encourage bike lanes on Indianola. As a cyclist, I love to see any improved safety options to encourage bicycling and reduce auto traffic. I support the Alt4 design. Thank you.
94. As a resident of Columbus, who owns a house near to Indianola and who rides my bike as much as possible, I'm writing to support the City's efforts to improve Indianola Avenues's access and safety particularly people who walk and bike frequently along the corridor. Improving the corridor for people who walk and bike will also improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrow (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its implementation. The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Continue the Kelso to Weber cross section through the Weber to Milford segment. In addition, transitioning from bike lanes to sharrow is a design appropriate for 1% of people and does not encourage new cyclists. Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.
95. As resident of the City of Columbus, I'm writing to support the City's efforts to improve Indianola Avenues's access and safety for all roadway users, including for people who walk and bike frequently along the corridor. Improving the corridor for people who walk and bike will also



improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. Alt 3 is the safest and most stress free. When ever I encourage friends and family to ride, they say they are afraid. Making safe, stress free infrastructure is vital to promote active transportation.

96. Bike infrastructure is needed for this city to grow. This will allow more people to use the existing protected bike lanes on summit to get downtown efficiently and safely. Please adopt option 4 and remove street parking so that cyclists can move about safely
97. Improving the corridor for people who walk and bike will also improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally
98. As resident of the City of Columbus, I'm writing to support the City's efforts to improve Indianola Avenues's access and safety for all roadway users, including for people who walk and bike frequently along the corridor. I myself am one of the people who frequently bike and walk along this corridor, because I live on Indianola. Improving the corridor for people who walk and bike will also improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrows (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its implementation. The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Continue the Kelso to Weber cross section through the Weber to Milford segment. In addition, transitioning from bike lanes to sharrows is a design appropriate for 1% of people and does not encourage new cyclists. Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and

accessible to all will increase business while making the corridor safe and inviting as a community space.

99. I'm writing in support of alternative 3 for the proposed bike lane initiative on Indianola. It needs to be made clear that bikes belong on the road. Almost every time I ride my bike through clintonville, campus, and downtown, I get yelled at, have had things thrown at me, and have had people threaten me. Our current bike infrastructure on Indianola puts bikes on rough parts of the road in close proximity to parked cars and in danger of being "doored". People ignore sharrows and park in the bikes lanes where they run alongside street parking areas. Buffered bike lanes keep cyclists and pedestrians safer, they encourage more people to ride bikes, and they help increase traffic to businesses. It can also eventually relieve heavy congestion as columbus continues to grow. Ultimately I hope to see buffered bike lanes all over the city, and im hoping to see the trabue/hilliard rome bike plan go into effect as well as the indianola plan. We shouldn't feel like we are risking our lives to commute by bicycle year round.
100. Please do everything you can to include as many miles of protected or buffered bike lanes as you can! This city desperately needs to reform it's hyper-car centric focus. Protected bike lanes when connected to a wider bike system are proven to increase bicycle ridership. Increasing car lanes does not reduce traffic, it is proven to INCREASE traffic. As you resurface these roads please make the right choice for our community and choose the safest and most protected bike options you can, for the most amount of roadway you can. Every Mile counts and the more connected we can make these lanes the better, safer, and cleaner our city will be.
101. Bike lanes should be more of a priority, extending the bike lanes further South on Indianola would benefit many individuals. It would provide a safer method of travel and promote a method of transport that has a much smaller environmental impact than motor vehicles.
102. As resident of the City of Columbus, I'm writing to support the City's efforts to improve Indianola Avenues's access and safety for all roadway users, including for people who walk and bike frequently along the corridor. Improving the corridor for people who walk and bike will also improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone.
103. having more bike/pedestrian friendly roads is a huge first step in cutting carbon emissions and make the city more inclusive over all.
104. I first want to disclose that I live in this area and previously worked for the Department of Public Service Division of Traffic Management and for Justin Goodwin, and currently work for Central Ohio Transit Authority (COTA) as a project manager. I have long desired to see changes to this portion of Indianola Avenue that would slow vehicular speeds, improve safety and pedestrian comfort, preserve on time performance for transit vehicles, while bringing a roadway that was designed with vehicular level of service in mind back to a more livable, inviting, neighborhood scale. Over the course of my career have been an advocate for right-of-way planning and design that serves roadway users of all ages and abilities, safely, and equitably. I am also a parent whose 2nd grader must cross Indianola Avenue to attend Clinton Elementary, and eventually, Dominion Middle School on Arcadia. Below are reasons I am in favor of the City's preferred Alternative (#4) or other new alternatives that reduce vehicular travel lanes to one lane in each direction while adding dedicated bicycle facilities: 1) Safety and Vision Zero

Crash data and the Safety Evaluation recommends road diet (decreasing number of lanes, and/or lane widths), parking alterations, signal timing, and other physical changes to improve traffic safety. A re-evaluation of parking design could help improve visibility and sight distances for motorists and non-motorists alike. Parking utilization is relatively low, and restricted during portions of the day in some locations, causing an unpredictable lane configuration for drivers and varying availability and utilization of parking for residents. As a resident of the neighborhood prior to and during this pandemic, parking demand is seldom moderate, let alone high, even when there are neighborhood events such as Crew games, the Shine On Festival, markets at SoHud, Crest Fest, holidays, etc. Additionally, when particular blocks are in demand, parking is easily found within a very short walking distance. Most homes have off-street parking available – this goes for single unit housing and multi-unit housing. Time-restricted parking along Indianola Avenue results in the following conditions: Inconsistent expectations for drivers who may not regularly see parked cars, especially outside of permitted hours. Drivers attempting to pass other drivers when both lanes are open, resulting in increased rates of speed, and unexpected braking due to inconsistently or sporadically parked cars. Passing and competing with other drivers decreases attention paid to parked cars, people crossing the street, or non-motorists. Drivers may not be able to see pedestrians as well behind cars parked, especially when parked too close to intersections. Enforcement of parking regulations is infrequent, resulting in less adherence to parking restrictions. Road diets have been shown to improve traffic safety and provide other benefits, including: Reductions in typical vehicular travel speeds. Reduced vehicular speeds improve the likelihood of survival if a pedestrian or other non-motorist are struck, and stopping distances are shorter for vehicles traveling at a lower rate of speed, increasing the changes driver will be able to stop a moving vehicle prior to impact. Rate of speed has long been a complaint along Indianola Avenue. The portion of the roadway north of North Broadway has shown reductions in crashes, crash severity, and typical vehicular speeds while continuing to maintain an acceptable level of vehicular service. Shortened pedestrian crossings – fewer lanes for pedestrians to cross and less risk of visibility issues for drivers due to multiple lanes in each direction. o Increased level of comfort for pedestrians, cyclists, people exiting parked vehicles in the right-of-way by slowing speeds, rightsizing lane widths, and creating or enlarging a buffer between vehicular travel lanes, parking, sidewalks, and possible bike and scooter lanes. Reduction in vehicular sound and vibration – slower streets are more livable and inviting in a neighborhood context. o Consistency in number of lanes and roadway design with Indianola Avenue north of North Broadway would create a more predictable, consistent roadway for motorists while achieving slower speeds, reducing the potential for and severity of crashes, while providing a more balanced allocation of public right-of-way for people of all ages and abilities. 2) Active Transportation and Recreation: Adding dedicated bike infrastructure along the stretch of Indianola from North Broadway to Hudson would complete an approx. 7 miles of continuous bike lanes from Indianola Avenue at Morse Road down to Mound Street, providing a continuous corridor for bike and scooter mobility. This study area is the last ~1 mile section to connect the existing bike lanes and cycle track on Summit and 4th to bike lanes north of North Broadway, providing increased buffers to parking zones, sidewalks, homes, and transit stops while increasing connectivity between Morse Road and Downtown. Increasing right of way for non-motorists using ADA equipment, cyclists of all ages, and pedestrians, especially where sidewalks are subpar, cluttered with debris or refuse containers, parked over by vehicles, or are nonexistent. Bike lanes, coupled with a road diet, improves the user experience and accommodation for everyone sharing the right-of-way while reducing

speeds and improving safety. I have often seen people using ADA assistive devices or pushing strollers in the bike lanes north of North Broadway as well as along Summit and 4th... especially where sidewalks are in poor condition, dirty, or flooded. Reducing crossing distance across width and number of vehicular lanes improves safety for residents in the area accessing schools and other community resources. There are several schools serving students nearby the study area, and many students must cross Indianola Avenue from the east to access elementary or middle schools.

2 3) Livability & Long Range Planning: Indianola Avenue is classified in Columbus' Multimodal Thoroughfare Plan as an 'Urban Community Connector.' This revised long range plan and legislation provides general guidance on roadway design and context considerations to support a growing region's needs, provide mobility options while leveraging our city's existing infrastructure, and guide investment and design decisions going forward. Indianola Avenue was historically classified as a '4-2 Arterial' in the previous auto-oriented thoroughfare plan heavily influenced by highway design. A 4-2 Arterial called for 4 travel lanes, 2 in each direction based on roadway design practices in previous decades, resulting in too many wide lanes and high rates of speeding. Neighborhood-scale collector and arterial roadways can be successfully redesigned to provide the benefits listed throughout this letter. This can also be done while preserving features like parking where practical, resulting in more inviting and safe streets for both residents, commuters, and for travelers passing through. Number of lanes does not always equal reduced congestion or increased safety. Summit Street is a good example where a roadway configuration change was made to accommodate all users, parking was preserved, and vehicular travel speeds were reduced to enhance overall safety. Much higher volume roadways than Indianola Avenue, such as High Street between downtown and OSU's campus, have been altered to maintain vehicular travel while providing wider sidewalks, shorter crossing distances, and slowing down rates of speed between traffic signals. It doesn't make sense to maintain varying lane configurations along the same roadway when Indianola Avenue has been successfully reconfigured north of North Broadway, and there are existing chokepoints at Hudson Street and North Broadway. Indianola south of Hudson transitions to a two lane roadway with some traffic diverted to I-71 and Summit Street. North of North Broadway, Indianola Avenue narrows to a 3 lane section with bike lanes. It is counterproductive to design for increased vehicular levels of service and maintain all the current vehicular travel lanes for only approximately only 1 mile of roadway of a much longer corridor, considering its context and relationship to the surrounding roadway network. The Bicentennial Bike Plan has called for bike facilities on Indianola Avenue based on long range planning and community engagement years ago. We've also learned there is a latent desire for potential cyclists who do not feel as comfortable taking the lane on roads with 35+ mph posted speeds. Long range planning efforts in Central Ohio such as Central Ohio Greenways and Columbus Recreation and Parks planning also seek to supplement and connect on-street facilities, existing trails, and create a more robust network for active transportation. Hudson Street just east of I-71 will soon have a multiuse path on one side, and a sidewalk on the other side of the street to better accommodate and serve people wanting to travel east and west between Olentangy and Alum Creek Trails, as well as access Summit and 4th bike facilities. Failing to incorporate some active transportation accommodation during this resurfacing project limits access to other facilities around the city that have already been designed, funded, and built.

4) Health, Environment & Sustainability: Columbus Public Health, Columbus City Schools, as well as other organizations have long recognized and promoted walking and biking for trips, especially of distances < 1 mile for positive physical and mental health outcomes. Reducing single occupant vehicular travel

reduces carbon emissions and improves air quality, reduces parking costs and demand near downtown, and increases transit ridership. The 3 benefits of increasing transit use are many but specifically increasing ridership increases COTA's operational budget to improve frequency and types of services offered, while reducing traffic congestion by decreasing the number of low-occupant vehicles on the road. COTA is also moving towards hybrid and electric vehicles to reduce the already smaller carbon footprint transit riders have than lower occupant and single occupant vehicles. Transit oriented design and walkable neighborhoods support future population growth, and aging, in the Central Ohio Region. Higher density and mixed use neighborhoods offer an array of benefits including lower long-term costs to implement and maintain infrastructure, greater connectivity and access to community services like schools and parks, greater social engagement, and age and disability friendly environments where residents of all ages and abilities can access schools, businesses, groceries, recreation, and more. 5) A New Normal Finally, the notion of the pre-Covid return to 'business as usual' versus the idea of an evolving new normal. None of us knows what the near, mid, or long term impacts of Covid-19 will be on how we live, learn, work, commute, and participate in our communities will be. So far, in our neighborhood, we've seen new festivals and flea markets, increased outdoor dining and entertainment, as well as social changes in how we engage in our communities. People are working, shopping, dining, learning, and recreating differently than we did just two years ago, and I'm sure our community will continue to evolve and adapt. The once almost obligatory choice for many to own a car is no longer simple considering tradeoffs concerning costs and increased availability of remote work and learning. Remote options also do not erase the need for parking and storing vehicles or curbside management for deliveries, rideshare, and transit. This brings to light a more urgent than ever need to carefully consider roadway design when we have opportunities to, like a resurfacing project. We could position ourselves now to make adjustments to the status quo that will continue to support and serve current needs, increase the likelihood that on the fence cyclists and others will engage in alternate modes, provide much needed safety improvements and reduced speeds, and options for new residents who do not yet live here who desire walkable, bikeable, transit, age, ability friendly communities. Dedicating valuable right-of-way to car storage, while an important and contextual consideration, does not make sense in our neighborhood where parking is not a scare commodity and we have many other needs to meet.

105. As a resident of the City of Columbus and regular traveler on Indianola Avenue, I'm writing to support the City's efforts to improve Indianola Avenue. Increasing the safety of traffic and providing safe biking and walking options for people along this road will improve the lives of the people in the city who use the corridor for transportation, improve safety, help address the impacts of climate change by encouraging alternatives to carbon emitting vehicles, and create the groundwork for more economic activity along the street. I have studied walkability and neighborhood design and believe that our traffic policy needs to go away from allowing cars to dominate the road. Induced demand means that cars will fill as much roadways as we give them. Additionally, we know that the majority of parking spaces are empty the majority of the time. We know that the parking lot surfaces contribute to heating up our city, increases the effects of climate change, lead to water runoff contamination, and contribute to flooding. Where we have existing roadway, it should be used in a way that will best for the community. Reducing the amount of parking we rely on is important. I support Alternative 3 with buffered bike lanes and parking removed. Enclosed/framed sidewalks (where it feels like there is some sort of barrier on either side, often buildings on one side and trees on the other) are the most likely to make



people feel comfortable when walking. The same is true for bike lanes and having a buffer would allow bicyclists to feel safe when riding. Additionally, having narrower streets that further narrow at intersections would encourage drivers to be more alert and cautious while driving. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrow (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its implementation. The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Continue the Kelso to Weber cross section through the Weber to Milford segment. In addition, transitioning from bike lanes to sharrow is a design appropriate for 1% of people and does not encourage new cyclists. Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.

106. As a Columbus resident that frequently commuted on bike from North Broadway to 15th on Indianola Avenue, I strongly believe Alt 3 provides the best opportunity to improve the corridor for everyone. Peak parking restrictions make the current bike experience fine, but when there is parking Indianola becomes very risky feeling for biking compared to north and south of this area due to increased, faster traffic and limited space. Removing peak parking restrictions would make me avoid the area on my a bike. Alt 3 provides a safe option for all.
107. I would love more bike lanes in general, indianola is a great place to start.
108. Adding cross walks and protected bike lanes is absolutely essential. Without protected bike lanes people are at serious risk in this area and many people who may otherwise be able to commute by bike won't do it if there is a section of their commute that is not safe.
109. As a resident of the City of Columbus and regular traveler on Indianola Avenue, I'm writing to support the City's efforts to improve Indianola Avenue. Increasing the safety of traffic and providing safe biking and walking options for people along this road will improve the lives of the people in the city who use the corridor for transportation, improve safety, help address the impacts of climate change by encouraging alternatives to carbon emitting vehicles, and create he the groundwork for more economic activity along the street. I have studied walkability and neighborhood design and believe that our traffic policy needs to go away from allowing cars to dominate the road. Induced demand means that cars will fill as much roadways as we give them. Additionally, we know that the majority of parking spaces are empty the majority of the time. We know that the parking lot surfaces contribute to heating up our city, increases the effects of



climate change, lead to water runoff contamination, and contribute to flooding. Where we have existing roadway, it should be used in a way that will best for the community. Reducing the amount of parking we rely on is important. I support Alternative 3 with buffered bike lanes and parking removed. Enclosed/framed sidewalks (where it feels like there is some sort of barrier on either side, often buildings on one side and trees on the other) are the most likely to make people feel comfortable when walking. The same is true for bike lanes and having a buffer would allow bicyclists to feel safe when riding. Additionally, having narrower streets that further narrow at intersections would encourage drivers to be more alert and cautious while driving. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrow (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its implementation. The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Continue the Kelso to Weber cross section through the Weber to Milford segment. In addition, transitioning from bike lanes to sharrow is a design appropriate for 1% of people and does not encourage new cyclists. Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.

110. I am in favor of the Alternative 3 option for the Indianola bike lane. I live south of Arcadia in the Old North neighborhood and frequently bike to Clintonville to visit friends and go to restaurants and parks. Right now it is challenging to bike in that area, and I often end up taking much longer routes than necessary to avoid busy streets. A bike lane similar to the one on Summit would make it much faster and more enjoyable for me to get places on bike.
111. More lanes for bicycles and pedestrians is absolutely needed.
112. I have some comments regarding the proposed bike lane additions on Indianola Ave. from E North Broadway to Hudson. This is a 1.1 mile stretch of Indianola that according to the proposal will have 5 different lane configuration changes. Way too many changes considering the congested area. I believe these bike lanes will make this stretch more dangerous, especially between Weber and Midgard. The bike lanes between E North Broadway and Morse Road have caused more traffic issues and these bike lanes are rarely used. I believe the money for this project can be better spent by making improvements in the Linden area, more activity programs for the youth and more Police protection.

113. Please do alternative 3! Columbus needs to step into the future of car-free mobility options. Having option 3 would be the safest and make the most sense for bike and pedestrian safety
114. Two changes are critically important to make all traffic safer on Indianola Ave: 1. Lower the speed limit to 25mph. High speed is the most important contributor to crash severity. Currently the speed limit is 35mph, but the road surface is not very good, so actual traffic speed tends to be around or even below the speed limit. This will likely change after resurfacing. The Columbus Vision Zero crash data map shows several crashes involving pedestrians, bicyclists, and motorcyclists. Reduced speed is the single most effective change to reduce crash severity. (Paris, France, has now a city wide speed limit of 19mph/30kph!). Add more protections for vulnerable road users, in particular for pedestrians and bicyclists. Pedestrians frequently walk in the areas close to Hudson, Weber, and N Broadway, where businesses and restaurants are located. Crosswalks with lights, so that cars actually stop, would help, e.g. around Studio 35 (how often have I crossed N 4th at Maynard, and cars didn't stop at all). Adding dedicated bike lanes would protect bicyclists much better, and the road is certainly wide enough to accommodate them. Let's implement Columbus' Vision Zero!
115. I live in clintonville and would love to have bike lanes on Indianola. Right now there isn't a good way to travel north/south through the neighborhood.
116. I live in Columbus and frequently bike places. I fully support the City's efforts to improve Indianola Avenue's access and safety for all roadway users, including for people who walk and bike frequently along the corridor. Bike and pedestrian deaths have been increasing nationwide while climate change continues to escalate. Improving Indianola Avenue will help people and the planet. Thank you.
117. As a resident of the City of Columbus and regular traveler on Indianola Avenue, I'm writing to support the City's efforts to improve Indianola Avenue. Increasing the safety of traffic and providing safe biking and walking options for people along this road will improve the lives of the people in the city who use the corridor for transportation, improve safety, help address the impacts of climate change by encouraging alternatives to carbon emitting vehicles, and create the groundwork for more economic activity along the street. I have studied walkability and neighborhood design and believe that our traffic policy needs to go away from allowing cars to dominate the road. Induced demand means that cars will fill as much roadways as we give them. Additionally, we know that the majority of parking spaces are empty the majority of the time. We know that the parking lot surfaces contribute to heating up our city, increases the effects of climate change, lead to water runoff contamination, and contribute to flooding. Where we have existing roadway, it should be used in a way that will best for the community. Reducing the amount of parking we rely on is important. I support Alternative 3 with buffered bike lanes and parking removed. Enclosed/framed sidewalks (where it feels like there is some sort of barrier on either side, often buildings on one side and trees on the other) are the most likely to make people feel comfortable when walking. The same is true for bike lanes and having a buffer would allow bicyclists to feel safe when riding. Additionally, having narrower streets that further narrow at intersections would encourage drivers to be more alert and cautious while driving. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for

residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrow (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its implementation. The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Continue the Kelso to Weber cross section through the Weber to Milford segment. In addition, transitioning from bike lanes to sharrow is a design appropriate for 1% of people and does not encourage new cyclists. Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.

118. I live right on Hudson/Indianola where I see fellow bikers passing by regularly. Indianola and Hudson experience a lot of pedestrian and bike traffic, neither modes of transportation are currently safe with the present state of the infrastructure. There's little room for cars to pass and skinny sidewalks lined by overgrown trees push pedestrians into bike and car traffic. The intersection (Hudson/Indianola) is one of the most intimidating in this area and a bike lane would relieve much of this spatial tension. A family member lives north on Indianola and I avoid biking there because of the car traffic and lack of space. A bike lane would not only give bikers like myself room to safely travel (which should be a right in contemporary cities), but also create a cushion for pedestrians who are constantly forced to walk in the street.
119. Improving the corridor for people who walk and bike will also improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrow (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its

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120. I think the design is fine. But I suggest one of the intersections between Arcadia and Weber have either a traffic signal or pedestrian beacon. Also a signal or beacon between Weber and EN Broadway. Near a bus stop would be best. A friend of mine who is blind and lives near Indianola between Arcadia and Weber stopped riding the bus when the traffic signal was removed many years ago. I forget which intersection. An audible PHB would allow her and others to use the bus again.
121. As a resident of the City of Columbus and regular traveler on Indianola Avenue, I'm writing to support the City's efforts to improve Indianola Avenue. Increasing the safety of traffic and providing safe biking and walking options for people along this road will improve the lives of the people in the city who use the corridor for transportation, improve safety, help address the impacts of climate change by encouraging alternatives to carbon emitting vehicles, and create the groundwork for more economic activity along the street. I have studied walkability and neighborhood design and believe that our traffic policy needs to go away from allowing cars to dominate the road. Induced demand means that cars will fill as much roadways as we give them. Additionally, we know that the majority of parking spaces are empty the majority of the time. We know that the parking lot surfaces contribute to heating up our city, increases the effects of climate change, lead to water runoff contamination, and contribute to flooding. Where we have existing roadway, it should be used in a way that will best for the community. Reducing the amount of parking we rely on is important. I support Alternative 3 with buffered bike lanes and parking removed. Enclosed/framed sidewalks (where it feels like there is some sort of barrier on either side, often buildings on one side and trees on the other) are the most likely to make people feel comfortable when walking. The same is true for bike lanes and having a buffer would allow bicyclists to feel safe when riding. Additionally, having narrower streets that further narrow at intersections would encourage drivers to be more alert and cautious while driving. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone. I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrows (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is

not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its implementation. The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Continue the Kelso to Weber cross section through the Weber to Milford segment. In addition, transitioning from bike lanes to sharrows is a design appropriate for 1% of people and does not encourage new cyclists. Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.

122. As a homeowner on Indianola between Hudson and Arcadia, I would be strongly in favor of Alternative 3, buffered bike lanes with parking removed. Alt 4 as shown with the missing section from Melrose to Milford is effectively almost no different than what we have now. Furthermore, actual barriers are much more effective at creating safety for cyclists than painted lines. Cyclists also face the very real danger of "dooring" (running into a suddenly opened car door) with the Alt 4 plan as written. Let's take advantage of this opportunity to make our neighborhood safe for bikes.
123. As a resident of the City of Columbus, I'm writing to support the City's efforts to improve Indianola's safety for all roadway users, especially for people who walk and bike along the corridor. My home is on Indianola Ave. and I walk my dog along this corridor 2-3 times per day and have witnessed a handful of accidents & many near misses over the past few years. Improving the corridor for people who walk and bike will also improve the safety for street users, including drivers. Making this area more friendly to walkers & bikers will ultimately help business along the corridor. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Overall, Alt 3 provides the best opportunity to improve the corridor for everyone and Alt 3 is my first choice. If Alt 3 is not chosen, I will support the proposed Alt 4 project, but conditionally. My comments below address specific elements of the study and the proposed design (Alt 4). Study: The In-Person polling needs to survey residents along the corridor to determine if they would bike through the corridor based on low level of stress and increased connectivity. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Much of the opposition seems to revolve around the removal of parking on Indianola. As someone who lives on Indianola and travels this corridor frequently - there never seems to be many cars parked on Indianola. Also, the intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks with flashing beacons at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor. Alt 4 Design: When the bike lanes drop at intersection approaches, install super sharrows (colored paint to mark the shared bicycle lane) to increase the visibility of the infrastructure. A general sharrow is not considered bicycle infrastructure by many DOT's around the country. The City should discontinue its implementation. The design should be consistent through the corridor. Inconsistent design is confusing for all street users. Continue the Kelso to Weber cross section through the Weber to Milford segment. In addition, transitioning from bike lanes to sharrows is a design appropriate for 1% of people and does not encourage new cyclists.

Work with the community and business to develop a strategy to address any business owner concerns around parking. Again, business can increase by improving bicycle access. I appreciate the City's attempt at improving the Indianola Street corridor through Glen Echo and Clintonville. Making the street safe and accessible to all will increase business while making the corridor safe and inviting as a community space.

124. I use the present bike lanes for errands and longer destination exercise. I like the sense of safety and separation from cars and trucks. I hope they continue to exist and/or improve.
125. There should be protected bike lanes! As an OSU student who bikes in this area frequently having no buffer between vehicles and myself on a bike feels incredibly unsafe. Just putting a strip of paint on the road won't stop a giant car from slamming into me and killing me! Or a child on their bike, or anyone for that matter. Also, if there is to be street parking they should be flipped around so myself and other bikers aren't put into danger by people opening their car doors and pulling out of or into their parking spot without looking. A car centric design is not a complete street! Thank you for your time.
126. A protected bike lane would be so appreciated! I bike to my fathers house on indianola and Arden all the time and it would improve that stretch so much! I love how nice the one is into downtown and use it frequently.
127. Protected bike lanes are better than riding between parked cars and traffic. Place the parking near traffic and the bike lane between the parking and the sidewalk... Chicago has this through many of their main corridors.
128. Paint isn't infrastructure. Bike lanes should be protected lanes.
129. Paint isn't enough to protect cyclists on this stretch of road. Would like to see real protection and further prioritization of people traveling by bike and pedestrians!
130. I am very glad to see that bike lanes will be going in on Indianola. I know you are planning for the bikes to use Arcadia, but Hudson really needs a road diet and traffic calming, and this the perfect opportunity. Please consider something like the following:

Indianola to Summit	Summit to Glenmawr	Glenmawr to 4th
<-Bike	<-Bike	<-Bike
<-Car	<-Car	<-Car
Car->	<-Car (Left Hand Turn to Summit)	<-Car
Bike->	Car->	Car->
	Bike-> (if there's room)	Bike->

131. Alternative #3 is the best option for residents, the city, and the future of Columbus transportation. Having two buffered bike lanes will encourage more bike riding because it is the safest option. There needs to be a time where we consider biking to be one of the transportation methods of the future, and providing safe, protected bike lanes shows consideration for that future. We can no longer design for "now" and our current car-centric behavior, but we need to design for a future that is sustainable and that is attainable. Protected lanes will encourage a healthier and sustainable lifestyle for Columbus and its residents. An increase in bike riding will provide a decrease in driving, which has nothing but benefits (less congestion, less injury, less production of gasses harmful to us and the planet). Getting to that point requires the implementation of the safest and best biking options.
132. I like bike lanes! I use the upper Indianola lanes all the time. Traffic slowed a bit, but in a good way. Fewer people are driving to work. More are biking.



133. Please select Alternative 3. If we are serious about Vision Zero in this city, that's the best option.
134. I appreciate you folks making the effort to expand the bike infrastructure in the city. I'm writing in support of Alternative 3. For some time I used to bike from Blenheim along this stretch of Indianola, down to OSU. This stretch of road was hair raising almost every time as I never knew which car would come too close or hit me. As time allowed I switched to the Olentangy trail, but that turned a 20-minute ride into 35 minutes. One lesson I hope you folks heed is that 95% of a bike lane is not a bike lane. While I view physical barriers as optional, mixing with traffic at the N Broadway intersection and at the stretch between Weber and Melrose is concerning. Even if the buffer spaces are reduced, I'd like to see at least 4' dedicated to cyclists separate from the drive lane to minimize conflicts. I understand the concern the local businesses have about parking; I support local businesses, especially those integrated into neighborhoods and fronting the street, so I'd like to see this block thrive. I realize the median was installed only recently, but if the city would consider removing or narrowing that median, there might then be space for both a parking lane and bike lane on the southbound side. Otherwise, I think some creative work could be done to designate the portions of the side streets closest to Indianola as short-term parking and provide prominent signage indicating as much; I think a big blue "P" pointing to a couple stretches of side street might go a long way to alleviating the businesses' concerns. In addition, I am deeply in support of high-frequency bus and increased crosstown lines in order to reduce the need to drive. Thank you for working with the community to increase availability of bike infrastructure. Given the threat of climate change and need to reduce car usage, I'd support seeing additional funding and attention put towards these projects in the future.
135. I am very disappointed that Alt 4. has been chosen for Indianola Avenue, this is not a complete street or going to achieve the complete street mission. Don't prioritize parking over active transportation. People on bicycles will bring more business to shops along the road more than a couple parking spaces. This has been proven time and time again. No one will use bicycles on roads with "sharrows", we don't want 2 ton vehicles tailgating us. And even for the Como to Walhalla redesign, why is the bicycle lane next to the Drive lane when you can easily put the parking in-between the bicycle lane and the drive lane for protection???? Please reconsider this and choose Alt 3!! To achieve all of the mobility goals that Columbus has set out, Alt 3 is the only way we can get more people on bicycles.
136. There are options which increase safety for pedestrians and bicyclists, and if "Vision Zero" is actually the goal, then protected bike lanes are the only option that moves us toward the goal. When people feel it is safe enough to ride bikes, they will ride bikes more often. Studying traffic and bike use as they are today on the current roadway is not an accurate representation of the demand, because demand changes to meet what is available. The safest condition for drivers, walkers, and bikers, is to have the least number of cars on the road, with the lowest speed limits, and greatest distance between cars and people not in cars. A plan that is still prioritizing driving and parking isn't a step toward Vision Zero.
137. As a student who often bikes around the area, I would feel much safer having buffered bike lanes along these sections.
138. Option 3 (Buffered bike lanes, no parking) Let's stop being such a car-obsessed city and be a champion of bicyclist safety. People feel safe in their cars, and they should feel safe on their bikes. Both are legitimate ways to get around, and both deserve protection. Favoring the convenience of parking over the safety from death or serious injury of a bicyclist would be an

egregious oversight and an insult to the bicyclists of Columbus. Thank you for taking my comments, and thank you for taking the time to look into improving our biking infrastructure.

139. Protected bike lanes save lives, reduce emotions, encourage folks to get out of their cars and onto a bike reducing traffic congestion, help local businesses, and promote public health. Clintonville is one of the densest and most bike-able neighborhoods in our city. We need better and safer bike infrastructure. The city should reconsider its preferred option in favor for one with protected bike infrastructure.
140. This is great news! I appreciate the City of Columbus for taking the time to listen to the public on this project. As a resident of the area, I must express my preference for alt 3. A protected bike lane, just like the one on Summit, is a much safer alternative than a separated lane. Imagine a child biking to school, just inches away from a 4,000lb truck going 35mph. I believe a lane that is totally protected from traffic is the safest alternative. Although, I really appreciate the efforts the city is going through on this project, I do not want to see this done in a halfway manner! Thanks!
141. Not in favor of taking away on street parking on Indianola in favor of rarely used bike lanes. As a Clintonville resident who rides to work 1-2 days a week I appreciate some bike lanes but don't support adding to traffic congestion by reducing car lanes when the area is becoming more dense. Adding bike lanes won't encourage me to bike more I still plan to use my car more often. Also you can count bike riders using bike lanes on Indianola North of N Broadway but those people aren't net new riders they were already riding their bikes on Indianola just on the road or on the sidewalk. Stop using Clintonville as your pet project and put bike lanes somewhere else.
142. I prefer Alternative 3 (protected bike lanes), but I would be concerned about having enough on-street parking for the commercial strip where Studio 35 is located.
143. Although I am a bicyclist, I am against any resurfacing that eliminates on street (Indianola) parking between Como - Weber and between Tulane - Olentangy St -- unless public parking is added (like 1 or 2 lots) in their place. I am only in favor of accommodating cyclists if business parking isn't displaced onto adjacent residential streets. Possible solutions: remove the center islands between Midgard - Weber for more mixed space. Or use Calumet as an alternate cyclist route. And/or create 1 or 2 public lots a la Worthington or the Short North. (Why Clintonville doesn't have more (any?) public parking in its commercial districts baffles me.)
144. I would prefer alternative 3, alternative 4 is an inferior solution.
145. I am very supportive of the effort to put bike lanes on Indianola! But PLEASE make the proposal better at the North Broadway intersection. You can't just stop the bike lane there. That's where it's needed most!
146. Buffered bike lanes the whole way would be better than Alternative 4, even if it means losing parking. There are plenty of other places to park on side streets and in driveways and garages. People should not be relying on street parking. I don't like that the bike lanes stop at Weber and North Broadway. They need to continue all the way without stopping! It's scary to have to suddenly merge with cars at those locations.
147. I used to live on E. Como Ave and bike to work every day down Indianola so I'm very familiar with this area. Obviously I would prefer to remove parking and add full protected bike lanes. I understand the challenges with executing this plan but I still think that's the best approach to create a truly safe, fully connected network. My second choice would be striped, dedicated right-of-way for bikes along the ENTIRE STRETCH. As Justin and others are well aware, any break in a pedestrian or bike network reduces the chance that people will use it AND any

place where cars and bikes are intentionally brought into conflict after being separated creates MASSIVE safety hazards for bikers and increases the chances of a car crash as well. Thus, my deep concern with the proposed Melrose to Milford approach - This is BY FAR the most dangerous part of this section to bike in under current conditions BECAUSE car traffic will try to pass you dangerously and at high speeds before reaching the median and bikes are incentivized to stick closer to the parked cars, increasing their chances of being door'd. So you're not fixing the biggest problem in this stretch of Indianola - in fact, you're making it worse - because currently cars are used to bikes being along this stretch in the driving lanes and your proposed approach would take bikes OUT of the driving lanes (which is great) UNTIL reaching the most dangerous section of the road during which time you force bikes back into driving lanes. Failing to dedicate biking right-of-way between Melrose and Milford undermines this entire plan. That is the critical section of Indianola - it's the most dangerous for bikers AND it's the one place where cars and bikes are brought into direct conflict BUT your proposed plan doesn't address it. That's unacceptable.

148. I live in the area that this is being planned and I think alternative 2 or 3 is a better option than 4. Bikers need to be protected from the cars that drive 45+ mph down Indianola. It's also important for some parking to remain, due to the increase in retail along Indianola. Please consider changing from alternative 4 and pick either 2 or 3.
149. Alternative 4 makes the most sense. Removing all parking from Indianola is going to put considerable pressure on street parking for the residential streets in this stretch, and limit options for businesses that don't have parking lots.
150. As a resident of Tibet Rd, near these proposed changes, I believe the city needs to invest in protected bike lanes. While the situation on Summit is less than ideal since cars often pull into the bike lane to see in order to turn, a protected two-way lane is ideal. The businesses along this stretch of Indianola exist in a neighborhood that could be walkable if we invested in making it walkable, and protected bike lanes are a step in that direction. Safe, protected bike infrastructure will serve to increase non-bike traffic and further eliminate a need for parking. I myself have had multiple close calls biking this area over the last decade from cars passing too closely despite multiple lanes of traffic. If concerns about car volume win the day, may I suggest that an even better plan would be to invest in a dedicated commuter bike trail from this neighborhood into downtown along the existing rail infrastructure just east of Indianola like what has been built up with the Camp Chase trail through the West Side. The Olentangy Trail is a recreation trail, and it's path adds extra miles and an uphill climb into downtown compared to traveling on High Street which discourages bike commuters from using the two instead of the roads. Extending protected bike lanes onto Indianola and further south on Summit or building a more direct rail trail is needed for the safety of everyone, drivers and bikers, traveling this corridor into downtown or just getting around the Clintonville, east of Campus, and Short North neighborhoods.
151. Whatever you choose, please DO NOT replicate the sheltered two lane disaster on Summit. I quit riding it after three tries. During all three tries, I was almost hit by drivers not observing signage. I like painted bike lanes on both sides with or without parking.
152. Hello. We need protected bike lanes. I regularly ride Indianola from Morse to Weber and sometimes it feels safer to take a whole lane of traffic than to rely on the painted bike lane. Drivers often cut dangerously close and the bike lane is regularly obstructed with debris. A protected bike lane is a safer and more useable option.

153. I live on Tulane just east of Indianola and I am for keeping parking intact. It is needed for the residents of my neighborhood, many of whom (myself included) do not have garages or off street parking options. I am in favor of creating bike lanes but I also would like to see the businesses in the Studio 35 strip succeed because we finally have some really good ones and they depend on parking availability. Also, I would imagine that the easiest solution is also the least expensive and I know the city could put that money to good use in more needy neighborhoods. Thank you.
154. As cyclist and a driver, our city needs more infrastructure to support bikes and bike commuting. Due to the significant difference in safety and between protected bike lanes and buffered ones, I value the protected bike lanes more to avoid car side swipes. We truly need more protected bike lanes for our city to offer more safe and green infrastructure. - Clintonville resident who works downtown
155. I am writing in support of Option 2. I am a bike commuter who lives in Clintonville north of north Broadway. I ride my bike to work in the university district daily. When I started to bike commute I took Indianola for its speed and direct access to the university district but quickly became nervous by how much I have to navigate through lanes of car traffic in order to get around. I have since opted for safer routes that are less direct, longer to travel, and much slower, because safety is my priority. Please move forward with the second option proposed.
156. I'm a resident of Columbus who is an avid cyclist as a means for mobility throughout our city. I often bike in fear because of the lack of infrastructure to keep cyclists safe from vehicles. I'm writing to request that the Alt 3 option be implemented for the Indianola Avenue improvements. Our bike and pedestrian infrastructure is way behind others of cities that are similar in size, climate, geography, and topography. This is our chance to make our city less car dependent which is a matter of safety, public health, and environmental preservation. Not to mention, when these types of improvements are implemented, it's proven to boost the revenue of small businesses along the route.
157. I don't understand why the city is even considering changing The traffic patterns on Indianola. The few bike enthusiasts are going to ruin the esthetics of this historic neighborhood. Clintonville has always been a nice place to live because...
158. I drive my kids to school along this route everyday, and am in favor of option 3, with protected bike lanes in both directions. Eliminating parking on this stretch of the road makes sense. Currently in the mornings, even when parking is not supposed to be allowed, there is always a car or two there still blocking the roadway--not great to navigate, but also proof that a single lane can work. I've lived in other cities that eliminated parking for bike lanes and the results have been extremely positive. Please use this opportunity to create a safer, greener street.
159. First of all, thank you for your work on this project. I am very glad to see the study being done and users other than just cars/drivers' experience being considered. I fully support narrowing the street to one driving lane in each direction with a center turning lane -- whatever else happens, I hope at least this goes through. The Arcadia/Indianola intersection will be so much safer for turning traffic with the two lane plus center configuration. I also approve of improving the bike entrance onto one-way summit North of Hudson -- it isn't great right now and signage doesn't make it clear where you're supposed to go as a bike. I would LOVE to see protected bike lanes like on Summit Street, that would be my first choice. I do understand the compromise being made for parking in alt#4, especially in support of the local businesses. However, it is not acceptable to have bike lanes disappear into sharrows right at the most

dangerous parts of Indianola. Doing this will essentially eliminate the use of the bike lanes for anyone not comfortable riding in heavy vehicular traffic and frankly makes it less safe for those who do ride comfortably in traffic because if there is a bike lane cars expect you to use it making merging into traffic even more difficult. I don't know what variations beyond what was presented were considered, but I'd like to propose a few: Could the city pave the alley behind the business district and help build shared parking off the alley? Could the city provide no interest loans to affected neighbor's losing on street parking to build on site parking on their own properties off of their alleys? Would diagonal back in parking in the center lane (where there's no need for turning) near the business district work to make up for parking lost? What I like about the summit bike lanes is that they are protected by parked cars, even though each lane is only 3' (?) The combined width with two side by side feels more generous, allows for easy passing and riding side by side while talking if there isn't anyone coming in the opposite direction. I have ridden these with my kids and felt safe doing so. My complaints about them are that they are often obstructed by recycling and trash bins, and littered with broken glass and gravel. Yet I still choose to use them to commute from Glen Echo to OSU. The buffered bike lane between Indianola and Calumet on Arcadia is less good. It too is often filled with gravel and broken glass but is too narrow to ride side by side next to my kid on the way to school and the Calumet intersection is treacherous. It feels much less safe than summit. Indianola north of North Broadway is too narrow for a trailer and feels unsafe to ride with kids on their own bikes. It too often collects trash, gravel and broken glass. Passing distance should be 3' minimum, yet if a bicycle is riding in the middle of a 5' unbuffered bike lane traffic is potentially whizzing by at 35mph just 2' from your shoulder. Add to this having to merge into traffic and you have a recipe for disaster. Thank you for taking the time to consider my thoughts on the matter. I hope the committee will reconsider alt#4 and at a minimum keep the bike lanes continuous throughout. If not, I would really encourage the city to reduce the speed limit through the business district to 25mph like they have done on high street as it goes though old Worthington and provide many more marked crosswalks than even the proposal showed.

160. Just wanted to give some feedback on the proposed bike lanes on Indianola. I am in favor of the project and I think it will improve the pedestrian and cycling experience along Indianola and eventually Hudson tremendously. I bike the stretch of Indianola between Hudson and Arcadia frequently (I rode Indianola home from school for years before the Summit Street path), so I know it well. I would like to advocate for option 3 in the city's proposal — the buffered lanes in each direction with no parking. I'd also advocate for a fully painted lane (green lane) — whichever direction you eventually go with. I ride with my ten year old daughter to school at Clinton Elementary almost every morning. We leave Glen Echo and then ride the newish Arcadia bike lane to Calumet. Having ridden it with her both before and after the bike lane, I have to say that I think the bike lane actually makes that stretch of Arcadia more dangerous. The lane is too narrow, but its presence convinces cars and trucks — particularly big trucks and busses — that its fine to pass in close proximity at high speeds. Cars that once were cautious in passing us now speed past well within the 3' passing distance (which is way too close). Moreover, we often have cars cut us off while make the right turn onto Arcadia even though we're both signaling — so they're making a right turn at the exact time we're making a right turn and they swerve around us. Cars coming out of the cross streets - of which thankfully there are not many — often glide right through the bike lane without stopping in order to get a better look up and down Arcadia. The Arcadia bike lane is also often full of gravel and glass, which makes it unpleasant and even more dangerous to ride. My daughter is an experienced

cyclist and I know that riding that stretch of Arcadia makes her nervous. I would not take a younger cyclist on that route now. I think some of these concerns could be addressed if the lane were just fully painted rather than just striped. It would be a better visual cue to cars coming on the cross streets and more of a visual cue to cars on Arcadia. But the whole thing would be better if it were just a wider lane or a buffered lane so that cars could not pass so close. I would hope that new lanes would address this problem rather than repeating it. I ride the Summit St. route from Hudson to Lane every day both ways. Its fine when you're headed south and very dangerous when you're headed north. Again, cars glide through the bike lane so that they can get a look up Summit. However, with two way bike traffic on a one way street cars rarely think to look south to check for bikes. This means that if I'm riding North I stop if I even catch a glimpse of a car coming along one of the east west streets and I wait to make eye contact — If I don't get eye contact I just wait for them to go since they're oblivious of me. Its a lot of stop and start, which is hell on my knees, but better than getting run over. I would rather be riding Indianola north, but now that the bike lane is built on Summit I get cussed at almost every time I ride on Indianola. I've had bottles thrown at me twice. I know bike path design can't account for the culture or feeling of entitlement that drivers have in Columbus. These are the realities of biking in this city right now. I also get yelled at on Lane trying to get from Summit to campus — also because there is a bike path on Summit. Cyclists never need to go East/West I guess. Boy it would be nice to have a designated route between the 4th and Summit bike paths West to campus. It would also be nice if the cops could occasionally ticket the Amazon drivers who use the Summit street lane as delivery parking. They drive right down the bike lane leaving a cyclist no choice but to jump onto the sidewalk or squeeze between parked cars. I have no qualms about removing parking on Indianola. The four lane stretch between Hudson and Arcadia leads to a very dangerous intersection at the light at Indianola and Arcadia where cars making a left turn often come head to head with cars that veer into the left lane to get around someone making a turn. I wait a full three seconds before I ever drive through that intersection I've seen so many crashes there. I've only ever seen three or four cars park on the stretch of Indianola between Arcadia and Crestview. Perhaps the city could play nice by allocating money to help residents along that stretch improve their existing alleys and or repair/upgrade the garages they have. I think this could also be a strategy North of Crestview where the business community has some clear concerns about removing on-street parking. Residents get in a huff when they can't park on a city street in front of their houses, but they all have alleys and garages that almost none of them are using. I'm not sure why people don't use their alleys and garages, but maybe a survey could be done to find out. Its a waste, and if homeowners used them, it would free up a lot of on-street parking for local businesses while allowing for improvements to Indianola that would also help those businesses. I'm glad the city has taken this up.

161. Great idea.

162. I am writing in STRONG support of Option 2 – 2-lane protected bike lane. I know that there is hesitancy about making bike lanes \*protected\* -- but this is a very important feature of the improvement and I implore you to choose \*PROTECTED\*! It really and truly makes bikers feel safe from traffic, without having to worry about being sideswiped by passing cars (people texting on their phones or whatever). As a parent, the \*only\* situation I would allow my child to ride on a road is if it is PROTECTED. It's simply not safe otherwise in today's Car Culture of large vehicles (trucks and SUVs). There is a \*REAL OPPORTUNITY\* here to \*EXTEND\* and \*CONNECT\* the protected bike lane corridor from Summit further into Clintonville – let's not miss the once-in-a-lifetime opportunity! This would have a huge impact because it would make the Summit



portion of the path \*SAFELY ACCESSIBLE\* to a large portion of Clintonville and north Linden area (parts that currently can't get to it because they have to bike on busy car-only roads)! The benefit is a multiplier – if you add 50% more protected path onto the existing corridor, you will see more than a 50% increase in ridership! Investing in this new portion will actually \*GROW\* the benefit of the city's previous investment along Summit! Win win! From the 300 block of Tulane Rd, I am hesitant to ride to the Summit bike lane – not because it isn't safe (far from it – once I get there it's AMAZING) – but because there is not a good/safe way of getting there! (Without having to ride along Indianola during rush-hour). Another benefit of OPTION 2 = PROTECTED is to generally slow down car traffic on Indianola. Many studies have shown that people drive cars as fast as they feel "safe" – in other words, 5-lane straight shot roads (like Indianola north of Hudson) tend to have people driving quite fast! Certainly faster than the speed limit. This is dangerous for the pedestrians walking along the sidewalk that runs right along the road! It doesn't feel safe at all, which is why so many residents avoid walking along Indianola if at all possible. By designing the road in a way that prioritizes all modes of travel – pedestrian, bike, etc – people will naturally slow down their cars, and more people will be able to enjoy their walkable community. I understand that the biggest reason why you will choose against option 2 is parking. I **\*\*implore\*\*** you in the strongest possible way to reconsider. We should not be designing our cities around the needs of cars (this only encourages more car-driving)! We should be designing our cities around **\*\*PEOPLE\*\***! Car parking is not "free" – there is a definite \*COST\* associated with the City of Columbus choosing to designate space to park cars in (albeit a diffuse and difficult to quantify cost) – a choice that benefits a few people (and also encourages \*more\* car driving in our city) vs. the benefit of improving the life of many many people (and also encourages \*less\* car driving in our city). I know what will happen because I've seen it 1000 times before. The person or business that lives directly on Indianola will complain about losing their parking spot (they have "standing"), but what is harder to measure is the BENEFIT in the quality of life of all of the citizens of the entire community that will have a more enjoyable experience traveling around their community! We have standing too! It's harder to measure the benefit, and we aren't as vocal (because it doesn't impact us as directly) but it's a REAL TANGIBLE BENEFIT with GREAT VALUE. I also understand that some people will complain that the bike paths aren't being used enough to justify such an investment. Don't listen to them! These things take time. With further investment, and more time, things will continue to improve. More bike lanes north and south will lead to more ridership. Also, there is much more benefit than just bike ridership – the whole community becomes more walkable – slower car traffic, more enjoyable. Bike lanes in Europe were vigorously fought, but over time they have dramatically improved the quality of life for a great many people!

<https://www.fastcompany.com/3052699/these-historical-photos-show-how-amsterdam-turned-itself-into-a-bike-riders-paradise> I lived in Europe for 4 years and I can tell you that the benefit of these choices over time are real and significant. There are many places where cities have chosen to prioritize non-car modes of transportation. They were vigorously fought at the time, but now they are seen as hugely impactful. I lived in London, England and commuted to work daily on a \*PROTECTED\* bike path for 20 minutes every day and it was LIFE CHANGING. It really was! I got exercise, I was happier, I felt \*SAFE\*. It was amazing. For most citizens in Columbus, this isn't even an \*option\* because it simply isn't safe (cars kill bikers = PROTECT US WITH PROTECTED LANES) The choice you are about to make is relatively small (it's only one road really), but all of the small changes add up over time! The city has to make deliberate changes over time to make things better. We can do it! You can do it! What better place to do it than

Clintonville. Please please PLEASE put in \*PROTECTED\* bike lanes. And in general – do MUCH MUCH more to add bike lanes / pedestrian options to our city. (Also, buying sweepers to regularly clean the existing bike corridors would be a relatively low-cost way of improving ridership. Some paths are so cluttered with rocks/gravel that they're not safe for road bikes.)

163. I live near Indianola and Olentangy St. I regularly bike to my office near downtown and frequently walk around the neighborhood with my children. I would love to see a protected bike lane on Indianola between Arcadia and North Broadway, similar to what is on Summit St. south of Hudson. Separating the bike lane from car traffic makes a significant difference in the safety and comfort of cyclists. Additionally, the sidewalks along Indianola are very narrow and I do not feel safe walking on them next to car traffic, especially when walking with children. Putting parking and/or bike lanes between the sidewalk and car traffic lanes would be a huge improvement. Thank you for all of your work on this project, I'm excited to see the results!
164. I would like to advocate for the city's planners to reconsider their current plans for the Indianola Ave complete streets initiative. Please consider installing protected bike lanes. As a growing city committed to becoming more green, we need to provide cyclists with a safe, dedicated space to commute, explore the city, or travel to businesses. Currently, I fear our city is still privileging cars and their owners instead of fully committing to elevating biking from "hobbyist curiosity" to "staple of 21st century commuting." As is, our city's bike lanes are often a terrifying gauntlet of cars, with nothing standing in the way of a distracted driver hitting someone. To fully commit to greening our city, it will require giving up parking spaces and other entitlements that car owners have come to expect. Protected bike lanes will help make our city safer, will help our equity goals, and will give parking space back to the people (as opposed to serving as de facto storage for car owners). Thank you for considering my plea. I really want Columbus to be an innovative, 21st century city :)
165. I am writing to express support for an Indianola resurfacing that includes protected bike lanes. An option that includes physical separation from vehicle traffic will be the most safe and allow for all groups to travel the corridor. I am both a cyclist and driver in the area, and believe a fully protected bike lane configuration is badly needed. Including a connection via Hudson Street or some other route should be added to planning goals as well. We need to think on the scale of networks and not just streets. I usually arrive to this corridor via Summit/4th Street bike lanes and Hudson Street presents a very dangerous gap in this network.
166. I learned on Columbus Underground today that the City is considering plans to add bikes to the stretch of Indianola between Hudson and North Broadway. As a Clintonville resident, I'd like to say that I'm very opposed to Alternatives 2 and 3 because they call for the removal of parking lanes on Indianola. There are a few businesses on this stretch (Studio 35 & The Crest Gastropub) and I also have friends who live on Indianola Avenue. Businesses and residents alike rely on the parking on Indianola Avenue. And I know that at least one street near the Crest is permit parking only and their lot isn't that big. I fear that losing parking on Indianola could congest parking on the side streets. Please consider this as you make improvements to Indianola Avenue.
167. The city maps do not show the bike lane on Hudson W of Indianola. The Arcadia-to-Summit bike connection does not connect to the bike lane on Hudson, therefore bike lanes on Indianola are needed in addition to the Arcadia-to-Summit bike connection. Crosswalks are great, but need lights to make sure traffic stops, not like N 4th and Maynard, where nobody stops. Where traffic lights exist, pedestrian lights should always turn to go, not just when someone is pushing a button. I've seen too many people miss the buttons, wait a red-green-

cycle without anything happening and cross on red. Always go-turning pedestrian lights condition drivers better that pedestrians have the right of way and can always go. Make the turn lane a raised center with trees to help the trees make the street appear narrower and drivers slow down (or at least speed less). Avoid contraflow bike lanes like hell. Drivers in a one way street do not expect oncoming traffic. An oncoming bicyclist might surprise drivers, which is really bad for bicyclists because they are much more vulnerable than cars. This is already a problem on Summit, where drivers do not look in the opposite direction for bicyclists going N when turning onto Summit or crossing Summit. This leads to dangerous situations, as I can attest from personal experience. Often cars are waiting on Indianola to turn left onto Arcadia. It's probably a good idea to make the center lane a designated turn lane before the intersection. This might be a good idea for the opposite direction, too, and also on Indianola at Hudson. One needs to have a death wish to ride a bicycle on Hudson between Indianola and N 4th. Lanes are narrow, drivers are speeding, and do not pay attention to bicyclists and pedestrians at all. Hudson needs to change dramatically to be safe for everyone.

168. I am writing to respond to the 8.31.21 Complete Streets Advisory Meeting call for feedback. First, I would like to share my displeasure with how this process has unfolded. I will acknowledge that the 2020 CAC Mobility Survey and the July 2021 Sidewalk Survey of patrons in the 3000 block of Indianola occurred. That said, the paucity of outreach and communication with those of us who actually live, work and shop in the neighborhood under discussion is incredibly frustrating and disappointing. I have lived in Columbus for over 25 years, 18 of which have been in the corridor between Arcadia and Milford, the area under discussion. If not for the efforts of Eric Brembeck, owner of Studio 35, and coverage by Cliff Wiltshire and Clintonville Spotlight, my observations of this process lead me to believe that residents of this neighborhood would not have heard about this study until the repaving project started. Our block has put more effort into collecting signatures for a block party than the City of Columbus has put into neighborhood communications about the project. Over the years, I have travelled US 23 from Ann Arbor, MI to Waynesville, NC. The section from Hudson to Indianola and North Broadway is among the most narrow along this entire stretch of US Highway. Based on the current behavior of drivers through my neighborhood, further constriction of traffic without speed reduction and traffic calming (regardless of bike lanes) gives me pause. For my birthday this year, I received a new bike and love riding it around the city. During the summer months, I would ride Indianola between Milford and Tompkins one to two times a week. However, based on traffic and driver behavior on all of our major thoroughfares I would rather safely take the Bike Path and use side streets to get to my destination. Finally, I am also a proud longtime patron of many, many of the business in the 3000 block of Indianola. I've been a customer of Imperial Cleaners and Studio 35 since the 1990's, and have shopped Yeah, Me Too, Fibonacci, Savor Growl and Koko since they opened. For our household, having these businesses open throughout the pandemic has been a blessing. The explosion of new businesses in our neighborhood in the last year or so is wonderful to see, and we want to see them thrive. With that in mind, Alternative 2 and Alternative 3 would absolutely decimate our neighborhood commercial district. As I expect the bike lane project to proceed regardless, Alternative 4 is the least worst option from this process, if we want to preserve and protect small businesses in our neighborhood. In sum, I am disappointed with this process and skeptical of the outcomes. I expect more from elected and appointed officials.

169. I live on Hudson Street between 4th and Indianola. Walking and biking in the neighborhood has been dangerous, so I support the city's efforts to improve Indianola Street's

access and safety for all roadway users, especially for people who walk and bike frequently along the corridor. Making improvements on the street for people who walk and bike will also improve the safety for street users, including drivers. In addition, based on best practices, making a business district more walkable and bike friendly can increase business for small business owners. With that said, change can be difficult, and lack of understanding of potential benefits need to be better communicated to obtain community support. Even though I see accidents here almost every week nobody has asked residents directly what we think. In-Person polling needs to survey residents to determine if they would bike through the corridor. A temporary demonstration would provide a real-world opportunity for residents to experience the different infrastructure types. Project should clearly identify how the proposed design will satisfy Vision Zero goals. Parking study should evaluate the current use and capacity of side streets to offset potential parking reconfiguration on Indianola. Intermittent crosswalk locations confuse motorists and can make unmarked crossing more dangerous. Recommend placing crosswalks at each intersection through the corridor. This will significantly raise the visibility of each crosswalk and establish consistency through the corridor.

170. I realize I am writing at quite literally the midnight hour of the open comment period, but I hope I can share additional perspective about the proposals for Indianola. As a neighbor living just east of Indianola and just south of Weber, I'm pleased to hear about the city's recent efforts to study and reimagine and use and safety of Indianola. I am disappointed, however, that the conversation is so focused on parking, re-striping lanes to slow cars, and adding bike lanes, but does little to address the safety for pedestrians and nothing to improve the condition of the sidewalks. This is an ongoing concern of mine, and I have reached out to a number of city, neighborhood, and school leaders in the past. (I have included several of those correspondents on this email). In December 2018 I submitted a 311 request for a traffic study specifically around the safety of the Weber and Indianola intersection. Cars travel too fast, sidewalks are too close to the street, utility poles block visibility, and drivers rarely yield to pedestrians in the crosswalks. Until recently there was a utility pole so close to the southeast corner of the intersection that it had been hit so many times it was shredded, held together with metal binding straps, and barely standing. Cars and trucks regularly drive over the curbs and corners. Crossing Indianola remains the biggest barrier for residents, particularly kids, who wish to walk to school, work, or neighbors houses on the other side. Only recently have I allowed my kids, ages 9 and 11, to cross on their own, even though their schools, many of their friends, and parks they like to play at are on the other side. What came of my request many months later was the addition of No Turn On Red signs between certain hours. It's not enough and many people never even noticed the change and addition of the signs. Once again, I would like to request No Turn On Red signs at all times, and a button-actuated pedestrian only "ALL WALK" cycle at the light at Indianola and Weber and. This the only way to ensure that cars stop and pedestrians stay safe. As the Wellness Committee chair at Clinton Elementary school, I started a Walking School Bus for the students in the fall of 2019. Each week, adults led multiple routes from all quadrants of the school boundaries. I led the southeast quadrant, safely escorting dozens of kids to school. I spent a great deal of time coaching the kids on how to walk safely on the sidewalks of Indianola – single file, as far from the road as possible. Cars and buses whizzed by us at terrifying speeds with little room to spare. Utility poles and signs cause people to walk in dangerous zig zags down the sidewalks, and pitted concrete and broken curbs are ever-present trip hazards. I know from our infant and toddler days that pushing a stroller down Indianola is impossible – there are too many signs and poles in the middle of the sidewalk that make it impassable. There is no way a



wheelchair could safely navigate Indianola. In December of 2019, I invited city council and school board members to walk along with us. A couple of aides took me up on the offer, and I begged them to work together to figure out a safe way for kids to walk to school. You can see in the pictures below, that despite freezing temps, our Walking School Bus days were hugely popular – the group pictured was just one of six routes!



There are also no crosswalks between Weber and Arcadia. In the 14 years we've lived here, I've seen two people struck by cars as they tried to cross Indianola. It's unacceptable to expect pedestrians to walk north to Weber or south to Arcadia, just to cross a road that divides their neighborhood. I would like to see the addition of at least two well-marked, wide crosswalks somewhere in that stretch. These crosswalks should also have button-actuated flashing lights to make cars in both directions stop and wait for pedestrians. Crestview would be a good location for one of these crosswalks, as many cross there to reach The Crest Gastropub. Parking should be limited to commercial areas only. Most residential properties on Indianola have garages or parking for 2-4 vehicles. Parked cars up and down Indianola create visibility issues at intersections. During rush hour, cars swerve between lanes and around parked cars to speed past other cars and jockey for lead positions in traffic. This past May, I was sitting on my front porch when I heard yet another massive crash on Indianola. When I ran to help the driver, I found that he had hit a parked car at such high speeds that he pushed the parked car onto the sidewalk, lost a wheel to his truck, and came to a stop up the hill in a neighbor's front yard. One can see how dangerous it can be for pedestrians to be forced to walk so close to the road. Accidents like this are more common than you would think, and many residents don't park their cars on Indianola because they know the chance of their car being hit is pretty high.







To prioritize cars and bikes in this plan for Indianola without addressing the needs of pedestrians – including some of our most precious (KIDS) and vulnerable (ADA accessibility) – is nothing short of a crime. The sidewalks and curbs need fixed immediately, and the utility poles and signs need to be moved out of the pedestrian right of way. Tree lawns should provide a buffer between traffic and the road. Re-stripping the road to make it narrower and slow is a good start, but ALL modes of transportation should be considered. The current conversations that have been facilitated around Indianola improvements are a good start, but I hope you will consider a greater investment in this project to ensure safety for all.

171. I live on Glen Echo Dr and am very pleased to see the city working on making Indianola Ave between Hudson and N Broadway safer for all users. Our transportation investments have historically prioritized private vehicles, at the expense of other modes. We must ensure that the most vulnerable users including pedestrians, bicyclists, transit riders, and people with disabilities are provided with safer options on our streets. I am writing to express my support for Alternative 3, which would include dedicated bike lanes the length of the corridor. These bike lanes would make it easier and safer for me to bike with my family to destinations in Clintonville. I have a 4 year old and 1 year old, and we love to use the bike lanes on Summit and the Olentangy Trail. We occasionally go on streets where we share the car lane, but I always feel our safety is at risk without adequate bike lanes. Getting this access to the neighborhood, including playgrounds, the theater, coffee shops, restaurants, grocery stores, Walhalla ravine, friends' houses, etc would be wonderful for us. Not only is it great exercise, but it creates less vehicles on the road to cause congestion and pollution. I understand there is a concern about losing parking in the commercial district, which is what led the city to propose Alternative 4. While I appreciate that Alternative 4 is an improvement overall, I'm also concerned about forcing bicyclists to go from a dedicated lane to a sharrow. This could be the chokepoint that

discourages more riders from making the trip via bike. It is also dangerous to make bikes and cars shift to sharing space. The dedicated bike lane should be continuous throughout the corridor. Studies indicate that increased bike traffic can actually increase business. Also, the parking study should have looked more carefully at the residential streets in the commercial district to see if spots there would be available for patrons, decreasing the need for parking along Indianola. During a public meeting, a business owner suggested moving the bus stop outside the commercial area to open up space for parking. As a transit rider, I am concerned that this suggestion expects transit riders to walk further to get to their destination, while people driving cars are given door to door options? In general, it seems that people would be willing to park and walk a small distance so that transit riders and bicyclists have better access to the commercial district. I'm also pleased to see the proposal includes installation of crosswalks at some points in the corridor. For consistency and safety, there should be crosswalks at every intersection. This benefits pedestrians and makes it clear to vehicles that this is an area where pedestrians are crossing the street. The crosswalks should also be more than just white lines. There are great examples of colorful paint and designs on crosswalks to increase visibility and improve the aesthetics of the street and neighborhood. This also seems like a great opportunity to advance Columbus' Vision Zero plan, and it would help to see exactly how this proposal is aligned with that. I understand that it was determined that Hudson St could not be the connection between the Summit 2-way bike lane and the Indianola bike lane (using Summit and Arcadia makes sense). I know the street has a lot of car traffic, but we desperately need traffic calming. Cars drive way too quickly along a strip with pedestrians on both sides and absolutely no buffer between the travel lane and the sidewalk. Everyday when I walk on that sidewalk with my kids and my dog, I am seriously concerned for our safety. Slowing the cars down could make a big difference. Finally, I have a point about public process. I moved to the neighborhood in May and only heard about this project on a neighborhood facebook page in September. I think the city could have done more on the ground outreach in the neighborhood to get more community input.

172. While I'm thinking about it, let me add an idea that I've seen for the Melrose to Milford stretch that has a green raised median: Instead of having parking lanes and bicycles sharing the main driving lane, the median could be widened to allow for 60 or 45 degree diagonal parking, which should be possible if the parking lanes are removed and the driving lanes are changed into a bike lane, a buffer, and a driving lane for each direction. The best illustration I can find are the two attached pictures.





173. I am the owner of the property at 2997-3001 Indianola Ave. The businesses in that location depend on the on-street parking for their customers. I have nothing against bicycles, but I would hate to see car parking eliminated from that block. It could cause financial problems for the current tenants, as well as inhibiting my ability to rent my storerooms in the future. Currently, the CPA at the location needs parking for his clients. Also the wellness center depends on parking for her customers. If parking disappears, rents in the area will be depressed further and tenant quality will suffer.
174. First you must enforce all bicycle traffic laws just like you do for cars and motorcycles. Why not test and license just like the other vehicles. Talk about freeloaders!
175. As someone who regularly uses Indianola to commute by bicycle, a dedicated, buffered bike lane would make me feel safer, especially during the 4-5 pm commute home (north bound). Columbus is at a crossroads and any time it can signal away from the automobile's supremacy it should take that opportunity. Public transit and more bike infrastructure is needed if we're ever to going to be a dense, urban city. The more you cater to cars the more you strengthen the people's reliance on them. I would be happy to discuss this further with you.
176. Thank you for this rare opportunity to comment on a specific neighborhood road. 1. Indianola Ave. between Weber and North Broadway has some of the worst pavement deterioration in the entire urban area — disgraceful lack of road maintenance in evidence ! 2. In this area where parking and traffic is congested, placing those islands with trees was the



absolute wrong idea. 3. If you decrease parking on Indianola Ave. the merchants will suffer and so will their customers — plus there will be more parking and traffic on the side streets which will upset the homeowners. 4. If you want to build a bike lane and have a nicer streetscape on Indianola Ave. then build a parking lot or parking structure in the area — otherwise, forget it.

177. I have been a home owner on Milford Ave near Studio 35 off Indianola for over 30 years. We have seen a huge influx of street parking in our neighborhood since the theater and several new businesses have opened on Indianola. City council has allowed reduced parking capacity for all of them and their patrons are parking on the residential streets; taking up precious parking spaces we need. Not to mention the speeding cars and area traffic increased when there's an OSU game or wine tasting or a festival. We have tolerated this as it's occasional and we want the businesses to do well also. To remove ANY parking on Indianola on any side would increase this parking unavailability and push that need further into our neighborhood on a daily basis. I think the bicyclists could detour to Calumet and be safer anyway. We have run out of street parking in our neighborhood and feel the bicycles have taken a priority over the tax paying residents of the area this would impact. Please keep in mind that any changes in parking will have a domino effect on our peaceful neighborhood.

178. In case it isn't too late to comment, I'd like to add my thoughts as someone who walks, cycles (for commuting), rides the buses, and drives along that stretch of road. The worst option from a cyclist's point of view is to put in disappearing bike lanes. If a cyclist needs a bike lane to ride that stretch even a small part with no bike lanes (sharrows don't count) makes the rest of the lanes useless. I saw the problem of disappearing bike lanes when I recently rode from downtown along Parsons to go to a meeting at the old ECOT building on South High near 270. The on-again-off-again bike lanes were frustrating, and because they aren't consistent, few cyclists who need the lanes would ever use the ones that are there. I understand that you need to make compromises, but please understand that if you build disappearing bike lanes, you are not serving the needs of cyclists who depend on cycling facilities. Instead, you are using the lanes to facilitate a configuration that works better for motorists and pedestrians. Maybe that's the best you can do, but on-again-off-again bike lanes are sometime you do *\*to\** cyclists, not *\*for\** cyclists.

179. From a long time cyclist in Clintonville, please don't put in a buffered two way Lane like we've got on Summit. Anything but that! It's the most dangerous place to ride in Columbus in my opinion and I avoid it at all costs. I could go on, but will spare you the rant. One-way lanes on each side, sure, but please no two-way lanes. Of the suggested options, sharrows are my favorite method.

180. Please do not reduce parking on Indianola from Broadway to Weber Rd. It is so hard to find a space now to visit the businesses that I fear many of them would lose customers and therefore have to close if this happens. The bike lane on Indianola north of Oakland Park is rarely used. Not many residents choose or are able to use bicycles to commute.

181. Hello I am a resident that lives on Indianola Ave. right beside studio £.35 and some of the other wonderful local businesses here in Columbus. I believe the illumination of the already limited on street parking would greatly impact not only some of the residents that already have limited parking but also these businesses. I would hate to see more local businesses lose the ability to accommodate guests in this area. While I am all for Weiker safety I believe there's better ways to remodel the road which allows for safer biking commute and on street parking for residences of business is a long Indianola Avenue. Please consider not removing the already limited spaces we have. Thank you.

182. I'm a resident of 3229 Indianola Ave. and recently read an article in the Dispatch about the proposed bicycle lanes. I would like the city to retain street parking on Indianola because it is used primarily by guests of residents like myself and we rely heavily on the availability of parking for friends and family. Retaining street parking will also prevent people who aren't local from encroaching on off-street parking for nearby cross streets and residences. The local businesses on Indianola rely on street parking for their customers, and carryout options like Papa Georgio's, Fibonacci's inside of Studio 35, Yeah Me Too coffee and more require temporary parking options for the majority of their business. I think an unintended consequence of removing parked cars from streets would be that through-traffic would 1) increase and 2) speed through faster, increasing noise levels and potential danger for residents crossing the street. Like it or not, despite its size and importance, Indianola is a neighborhood street, not an expressway. Cars are a fact of life, and parking is a necessary resource. I am fully in support of a bike lane for neighbors to explore the neighborhood, so long as it doesn't unnecessarily displace Indianola residences and business in the process. I would hate to see Clintonville become a parking desert. Thank you for your consideration and your time.
183. I am an avid rider in Columbus, and would like to weigh in on the proposal for the protected bike lanes. At one point, this [article](#) presents a seemingly valid argument from a business owner about the reduced parking in the area, worrying that this would reduce traffic to their shops. However, I think this argument misses a key point - street parking is an inefficient way to drive business to a shop. This argument is predicated on a key assumption/observation: Indianola is not a destination in and of itself. People treat it like a parking lot. I never think to myself, "boy, I sure would like to walk down Indianola today". I go because I need something specific from a specific business. Think of it this way: Imagine a person living Downtown, and they want to drive to the Kroger on High and 7th. Imagine driving along High Street during peak hours. What do you think your chances are of, say, spontaneously stopping at the Simply Rolled Ice Cream shop along the way home? The answer is zero. No one in their right mind would take an additional 5-10 minutes finding parking 1/4 mile away to walk a combined extra 1/2 mile, just to satisfy a whim. A car isolates you from your surroundings, which is nice for the driver when the environment is construction and traffic congestion, but it's bad for business owners. You know who would be way more inclined to make a spontaneous stop? Someone on a bike who only has to hop off, lock up directly outside, and walk in. I know a lot of cyclists in the area. What do you think of their chances of stopping at Simply Rolled on their way home from Kroger are? Also zero because they don't bike on High Street. They go a couple blocks over to Summit and cut through side streets. Summit defacto becomes a destination for cyclists who are simply getting from A to B, and let me tell you what: cyclists are hyper aware of their surroundings and they have money. Have you seen how much they spend on their bikes? The fact that pedestrianizing and adding bike infrastructure increases retail effectiveness is not an alien and untested concept. Given that Indianola acts as a defacto parking lot for the surrounding businesses (as the owners in the article seemingly concede), this situation is very similar to what happens when cities elect to remove mandatory minimum parking requirements. As an example, here is an [excerpt](#) from an FAQ from Strong Towns, an organization that provides reviews of up to date research about the many plights that surround urbanism and the troubles of sprawl, car dependency, and municipal fiscal health (I highly recommend the titular book). Refer to the links in response to the first question ("Parking needs to be convenient for my customers, or else I'll lose business") for the research backing up the claim that cyclists and pedestrians will drive business, particularly [this report from OTREC](#). It is a fairly easy argument

to grasp when you consider that a business thrives when they can effectively attract unique customers. That's pretty easy to do when potential customers voluntarily cycle 10 feet past your store front.

As a concise summary: 299 parking spots is nothing compared to the capacity of two lane bicycle paths and the space efficiency of parking a bicycle. It is more convenient to hop out of your bike than it is to hop out of a car. If you build a bicycle path, people will come to Indianola without a specific purpose in mind, driving unique customers to retail along the strip.

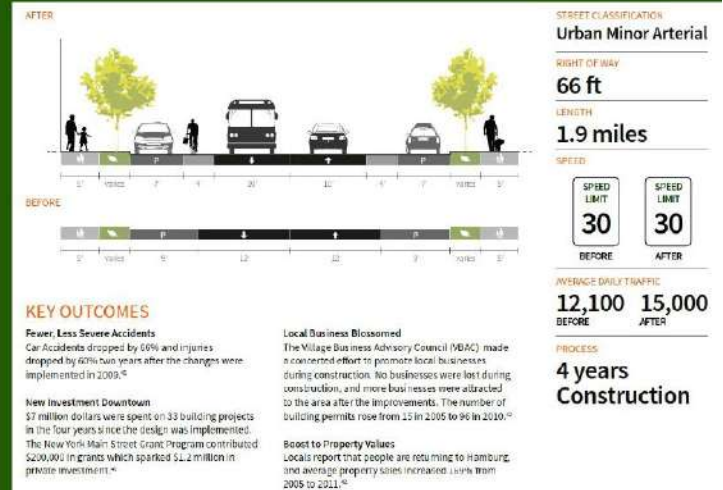
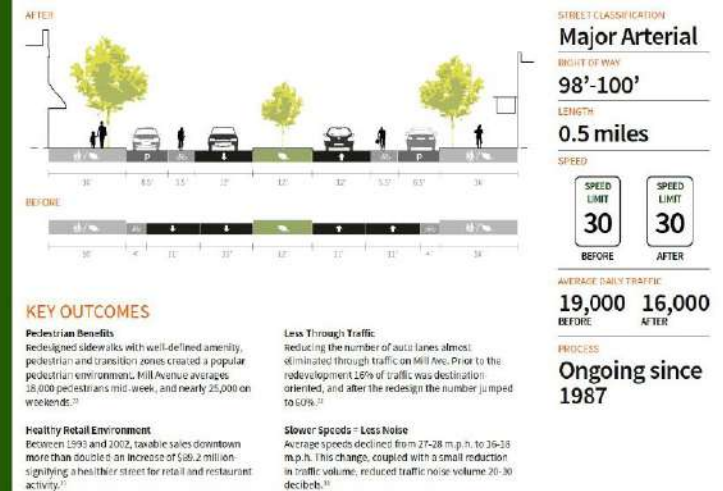
184. My wife and I own property on Indianola Avenue. We are very much against the elimination of street parking. To eliminate street parking would create a hardship for many residents and business owners. There is an enormous number of people that live on Indianola Ave between North Broadway and Hudson. Most of the property is multi family housing. Today I counted 104 multi family properties between North Broadway and Hudson. 33 of the buildings are 4 units or more. (A couple buildings actually have more than 10 apartments.) That adds up to at least 300 apartments. More than one person lives in many of these apartments. The vast majority of these residents have vehicles. The residents in the single-family homes up and down Indianola Ave also of course have vehicles. Many of these buildings were constructed in the 1930s with limited off street parking. This stretch of Indianola Ave is not like Indianola north of North Broadway where there are large off street commercial surface parking lots. It is illogical to eliminate any street parking in such a dense residential area. Why create another intractable parking situation like is present in the short north? Where are all these residents up and down Indianola, their guests and service providers going to park vehicles? Does the city have plans to provide alternative parking? I am a long time cyclist and support bike lanes where it makes sense. It does not make any sense to eliminate street parking for bike lanes on Indianola Ave between North Broadway and Hudson. Please use some common sense!
185. As a resident of indianola, I would still like there to be parking on at least one side of the street as many residents still use street parking for their vehicles. Also if parking moves to one side, there should be no parking time restrictions.
186. I am following up a previously sent email about improving Alt 4. This proposed Alt 5 is keeping more parking spaces while adding more improvements for pedestrians and bicyclists. Any disadvantages should be minor, and all suggested changes for Alt 5 already exist elsewhere in the city and should be no problem to implement. Details of how Alt 5 could be implemented are attached. From my conversations, Alt 5 has good support from diverse people in the neighborhood, including residents, businesses owners, and advocates. I want to highlight the positive feedback from two people in particular, Eric Brembeck, the owner of Studio 35, and Josh Lapp, of Designing Local, who has city planning experience. We would like to see all efforts to go toward making the redesign as good as possible, and we believe that Alt 5 is a step further in that direction. Please do not hesitate to reach out to discuss the redesign further!



# Capacity Analysis

3-lane configuration can be supported by each segment, Weber and Arcadia intersections

- Indianola has ~10-12k vehicles/day (MORPC), which can be supported by a 2-lane configuration (see examples from Rethinking Streets, right)
- Left-turn lanes are usually added at >300 turning vehicles/h, actual traffic into neighborhood cross streets is likely much lower, no center turn lanes needed (lower speed limit helps turning, too)
- Separate left turn lanes are only needed for intersections with more traffic (the Hudson to Olentangy segments of Indianola do not have left turn lanes or center turn lanes)
- Parking capacity is important for businesses
- Bike lanes and pedestrian facilities improve overall traffic by reducing vehicles



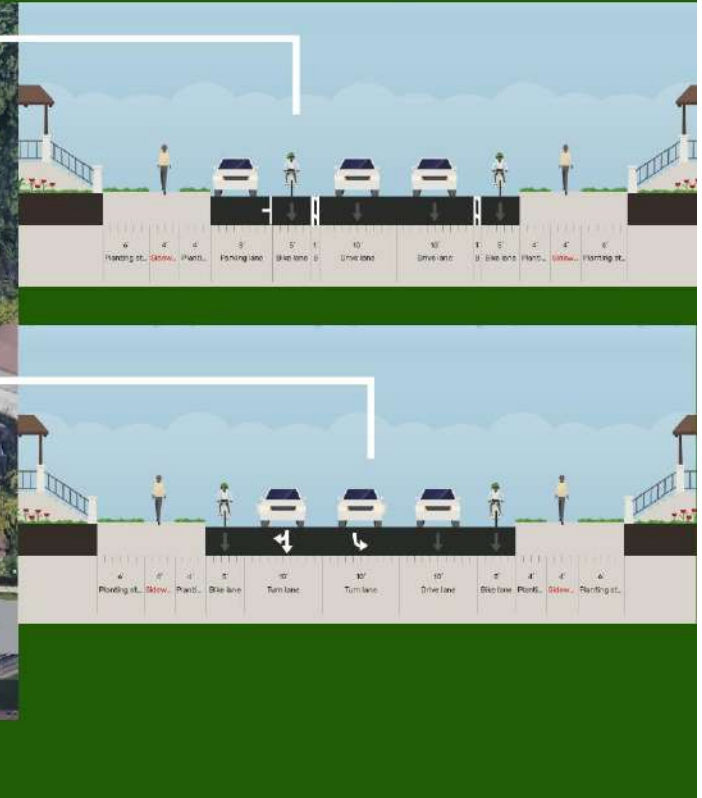
# Alt 4. Arcadia transition



# Alt 5. Keep parking on one side



- Alt 4. plus
- Designated turn lane
- More parking spots than Alt 4



# Alt 4. Cliffside to Tibet



# Alt 5. Parking on both sides



Alt 4. plus  
→ Keep the same number of parking spots as today



No parking on the bridge



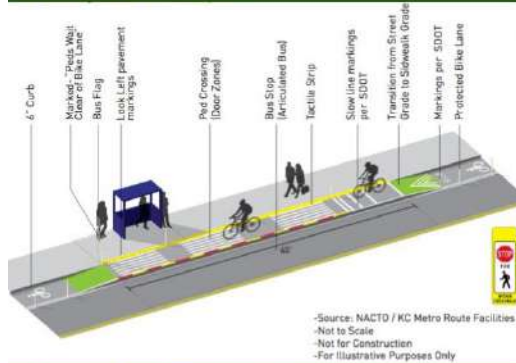


(no details for Alt 4.)

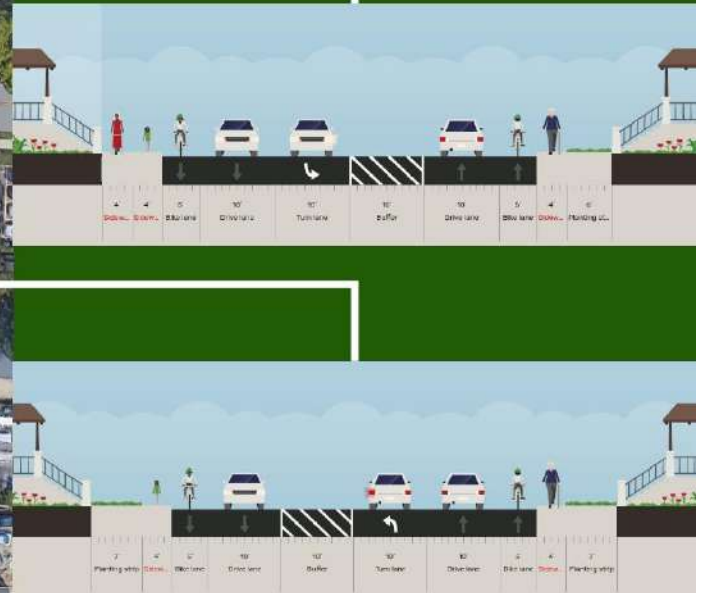
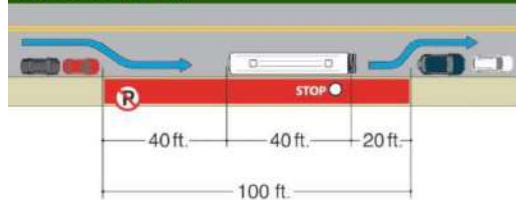
# Alt 5. Weber intersection

- Alt 4. plus
- Better bus stop design (not moved back, easy for bus b/c no need to turn, stopping bus stop traffic is safer for pedestrians)
- Dedicated turn lane

Bus stop designs:



Above: NACTO street design for shared bus stop (better)  
 Below: In parking lane (like now, but moved back)



# Alt 4. Weber to Midgard



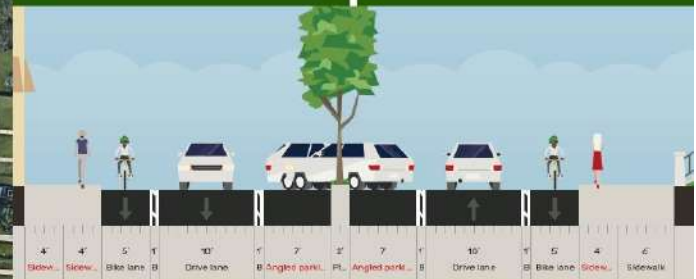
## Alt 4. downsides:

- No bike lane in the busiest part of Indianola
- Reduction in parking around can lead to more parked cars here
- Not clear if pedestrian crosswalk uses the median as island

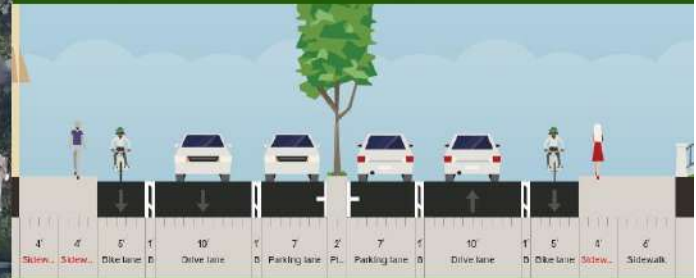
# Alt 5. Awesome-r for business and everyone



- Alt 4. plus
- Keep parking for businesses
- Traffic calming median, 25mph speed limit
- Pedestrian improvements
- Bike lanes
- Green median with trees



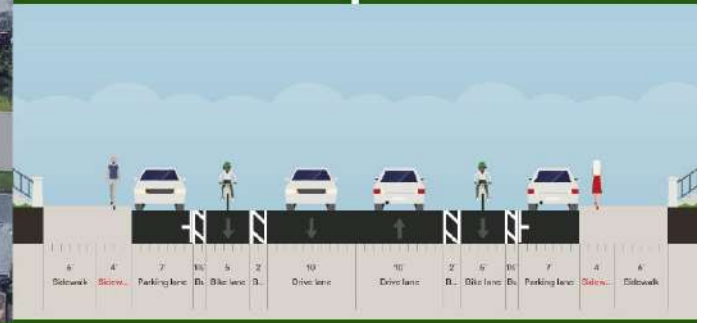
Parking could be diagonal or parallel:



# Alt 4. Midgard to N Broadway

# Alt 5. Parking on both sides

Alt 4. plus  
→ Keep parking on both sides  
(same number of parking spots as today)

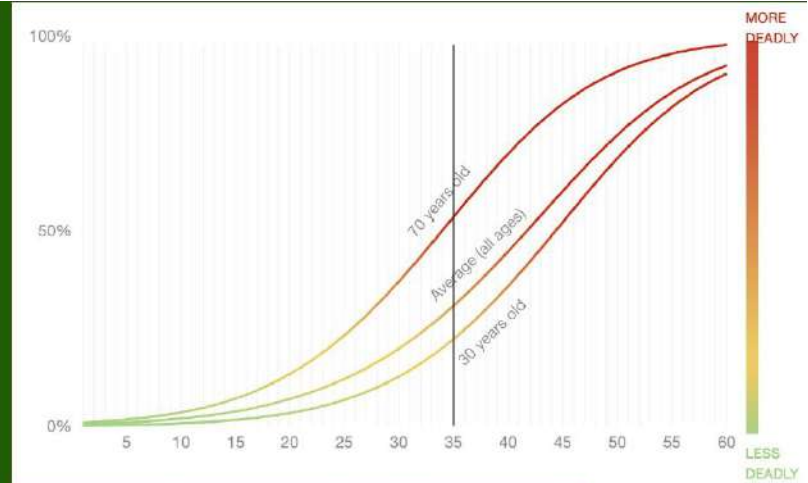




# Alt 5. Speed limit

Vision Zero goals: No fatal or serious injury crashes; equitable mobility; prioritize protecting human life

- Non-motorists (pedestrians, bicyclists, etc.) are the most vulnerable
- Distracted driving and excessive speed is the most-frequent causes for fatal accidents
- Street design (road diets, medians, pedestrian islands) and speed limits are the easiest way to reduce accidents
- Lower speeds can reduce bottlenecks, possibly improving the average speed
- Let's not use the 85<sup>th</sup> percentile speed, better use what speeds are safe for everyone to decide:
  - 20-25mph Weber to Midgard with many pedestrians
  - 25-30mph everywhere else
- Examples
  - NYC: 25mph, [link](#)
  - DC: 20mph, [link](#)



The Chance of Being Killed by a Car Going x mph.  
<https://www.propublica.org/article/unsafe-at-many-speeds>

With a speed limit of 25mph, driving the entire length of Indianola in the study area (1.22mi) will take 51s longer. That is without traffic and traffic lights.



# Alt 5. Modes of transportation

## Vision zero

- 30mph everywhere, except 25mph Weber to Midgard
- Lower speed limit lowers accident frequency, severity, and fatalities, it also makes lots of other things easier:
  - Turning on/off Indianola
  - Parking
  - Crossing the street as pedestrian
  - Lower passing speed is safer for bicyclists

## Treat other modes of transportation as equally important

- It doesn't even require the same amount of investment as side walks and bike lanes are cheaper to build

## Pedestrian improvements

- Crosswalks with lights and maybe speed humps?

## Bike lanes

- Marked in different color than pavement, e.g. green

Streets are safe, when you would let your kid bike to school. Bike lanes are great *if* they're not blocked. ([link](#))



# Alt 5b. Cycle tracks instead of buffered bike lanes

Cycle tracks plus buffer need the same road space as separate buffered bike lanes for each direction (5' + 5' + 2' buffer)

- Cycle tracks allow for more separation from traffic
- Cycle tracks need to move closer to the drive lanes at intersection to allow for better visibility (see NACTO design, below)
  - Allows more pedestrian space at intersections and shorter crossing distances



Car driving in the cycle track bike lane on Summit.  
Cycle tracks need to better prevent cars driving on it.

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## **Appendix B. Public Meeting Presentation Slides**

The presentation slides and meeting recordings were made available on the project website following the public meetings on Thursday, September 16, 2021.

# Public Meeting

September 16, 2021



THE CITY OF  
**COLUMBUS**  
ANDREW J. GINTHER, MAYOR

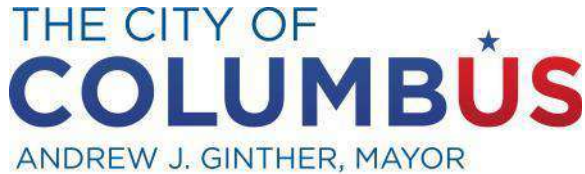
DEPARTMENT OF  
PUBLIC SERVICE

## INDIANOLA AVENUE COMPLETE STREET STUDY



# Presenters

- Donna Marbury, Warhol & Wall St.
- Kevin Dickens, Michael Baker International
- Emma Kogge, City of Columbus
- Justin Goodwin, City of Columbus
- John Ryan, City of Columbus



DEPARTMENT OF  
PUBLIC SERVICE



# Housekeeping

- Please mute to be respectful
- We know you may have specific questions about this project
  - You can type questions in the chat and we will make time to address them
  - You will have an opportunity to ask questions at the end
  - Use the “Raise Hand” function
- Send your more detailed questions after the meeting to [Indianola@columbus.gov](mailto:Indianola@columbus.gov)

# Agenda

- Project Purpose and Background
- Public Engagement
- Study Findings
- Conceptual Alternatives
- Next Steps
- Q&A

What is your relationship to  
Indianola Avenue?

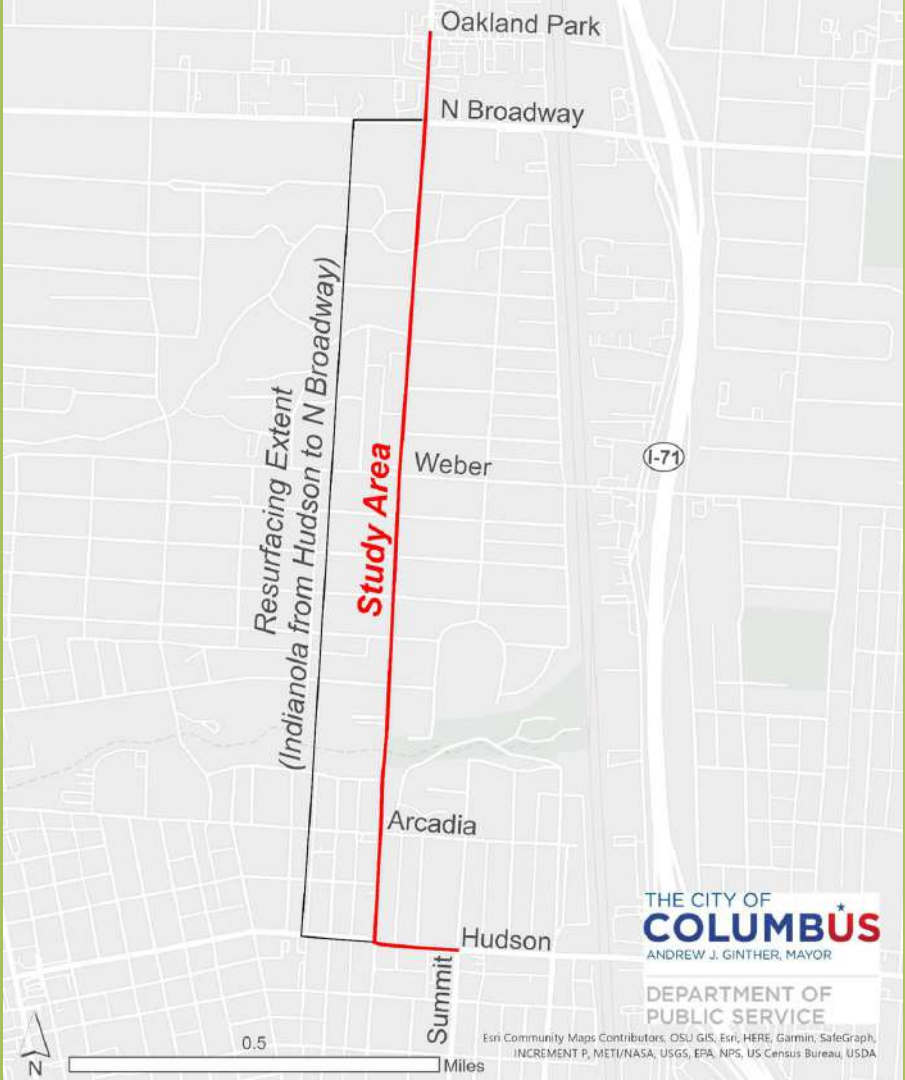
Enter your response in the chat



# Indianola Avenue Complete Streets Study Overview



# Study Area



# Goals for Complete Streets



# What is Vision Zero?

**More than 50 people die each year in traffic crashes on Columbus streets.**

- Vision Zero focuses on eliminating fatal and serious injury crashes;
- Promotes safe, healthy, and equitable mobility options; and
- Prioritizes protecting human lives.

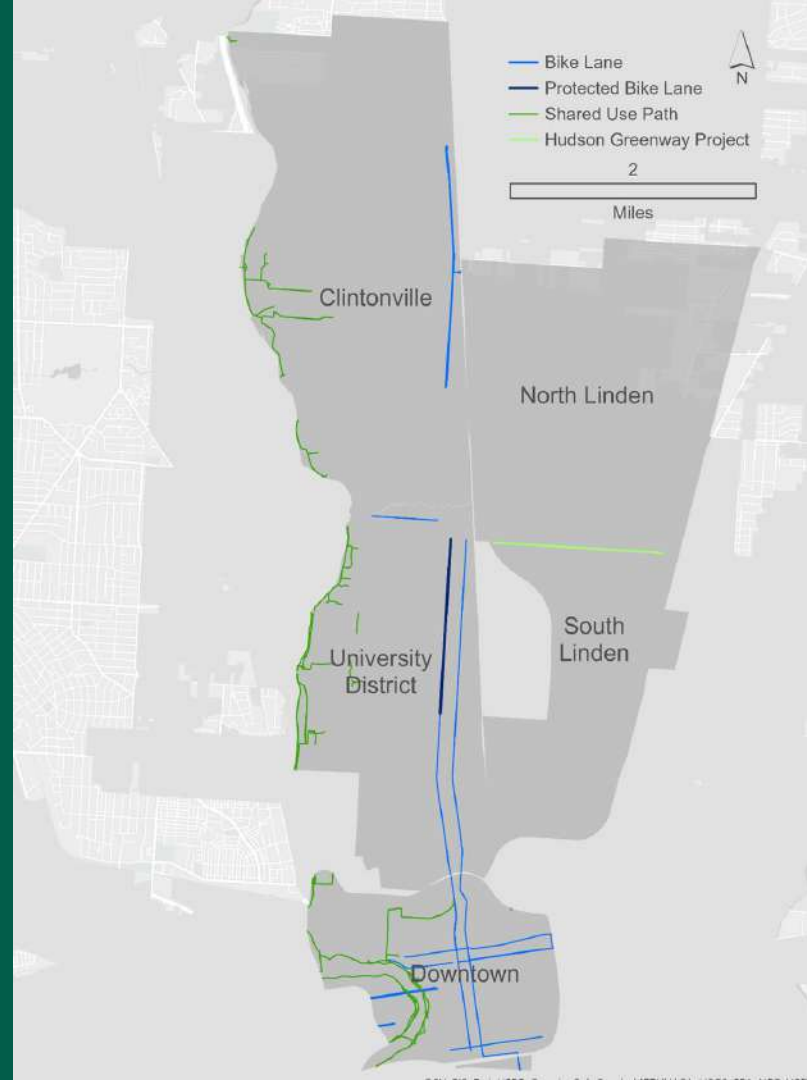


**ANATOMY OF A DANGEROUS STREET**

Source: Vision Zero Action Plan

# Bikeways

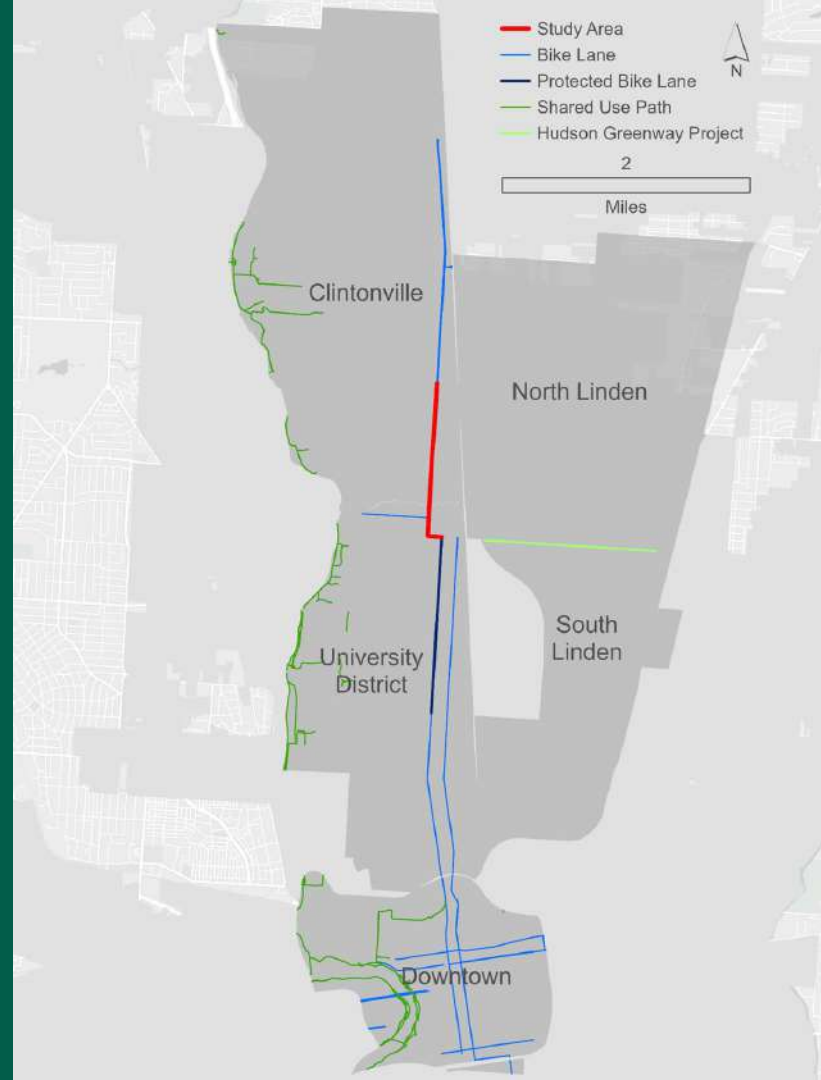
- Network gap between Hudson Street and Oakland Park Avenue





# Bikeways

- Network gap between Hudson Street and Oakland Park Avenue (study area)
- Seven miles of continuous facilities if filled from Downtown to Morse Road
- Opportunity to connect to the Summit Street cycle track





# Timeline (2021)





# Public Engagement Overview

# Public Engagement

- Advisory Committee
- Walk Audit
- In-Person Polling
- Clintonville Area Commission  
Mobility Survey Results
- Public Comments





# Overall Priorities



# In-Person Polling

- 70% drove and primarily parked on Indianola whereas only 55% drove when typically visiting
- Concern over losing parking for businesses
- General need for traffic calming; pedestrian, bike, and transit improvements



*Over 100 people surveyed over 2 days (July 2021)*



# Public Comments

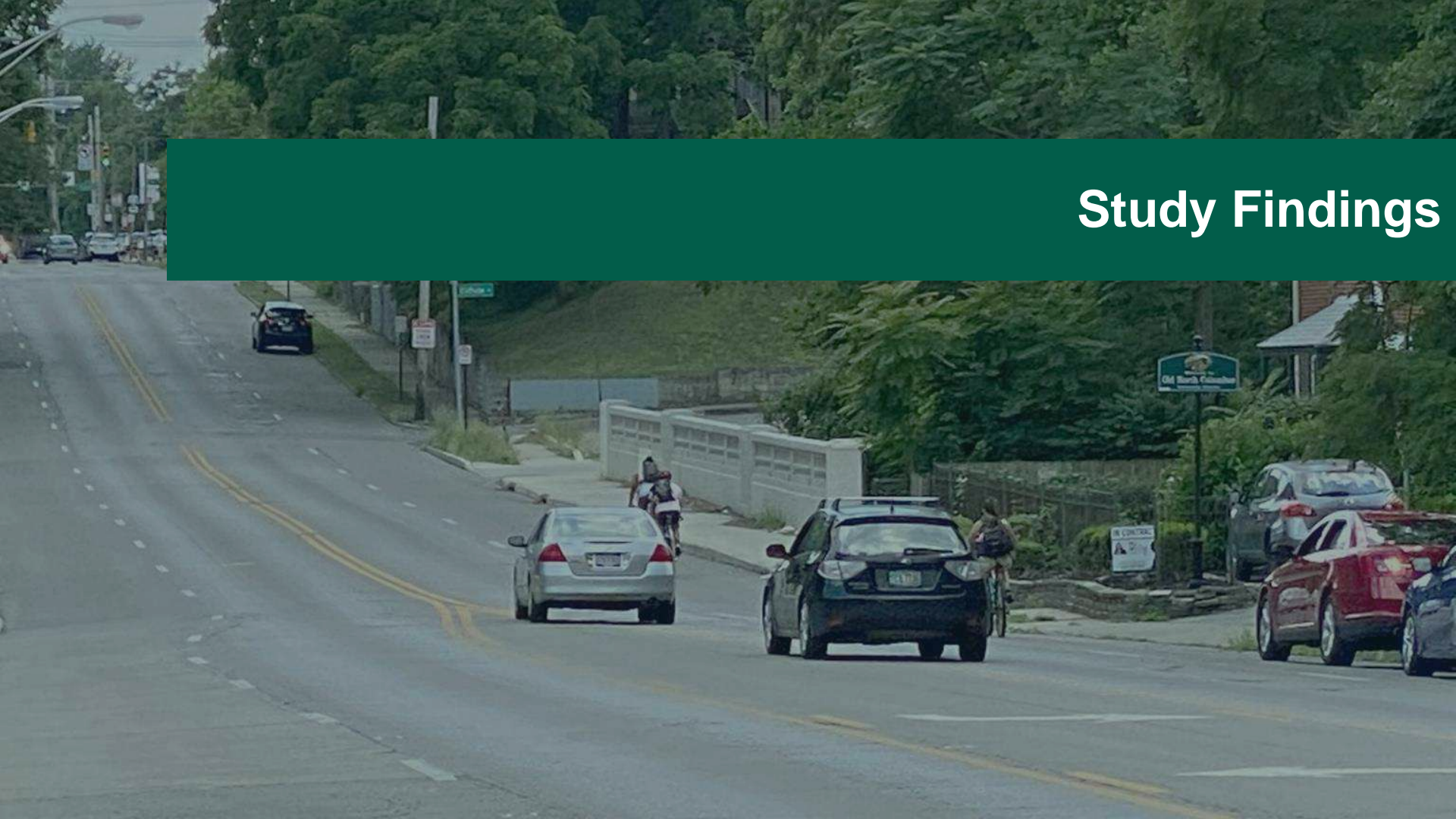
*“If there's a way this road stretch could have reduced speed, better traffic flow, and safer pedestrian walkways, that would be ideal!!”*

*“Please consider traffic calming.”*

*“I would love to safely bike on Indianola so I can reach the businesses around Indianola and Broadway.”*

*“Improvements to the intersections of Indianola at both Hudson and Arcadia would also be very welcome.”*

# Study Findings

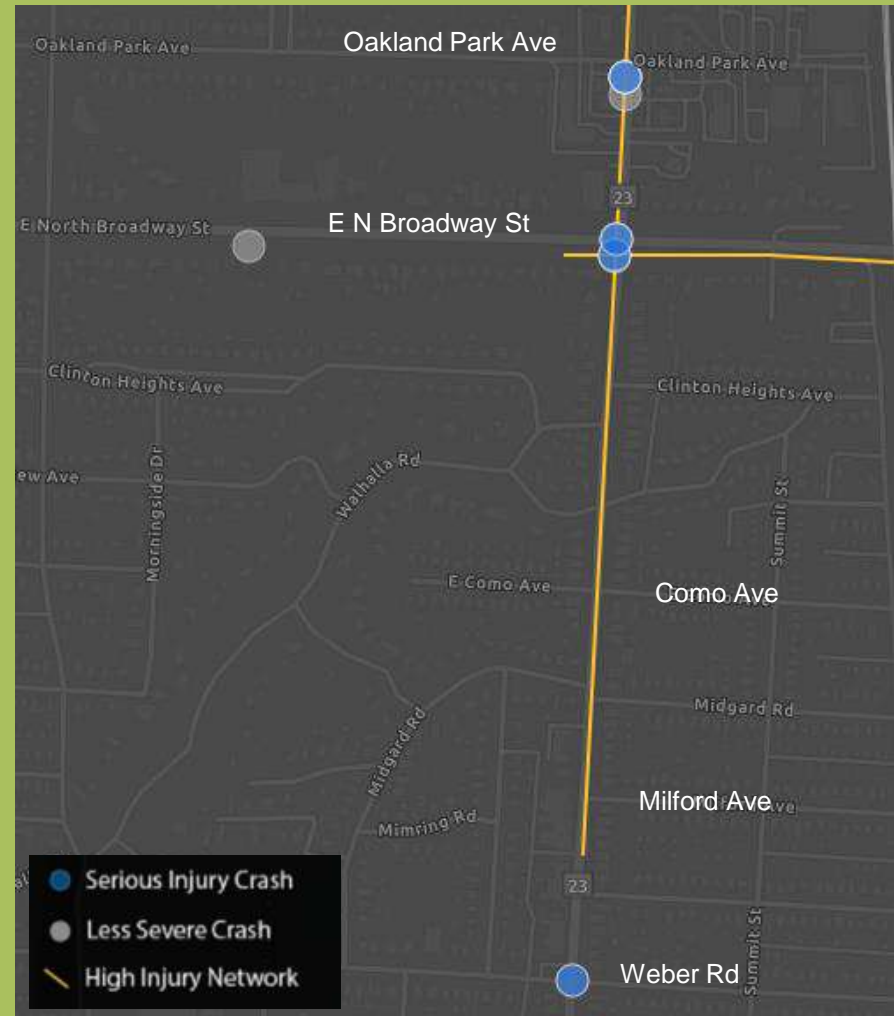


# Data & Analysis

- Safety review
- Parking utilization study
- Crosswalk analysis
- Traffic capacity analysis
- Previous lane reallocation results

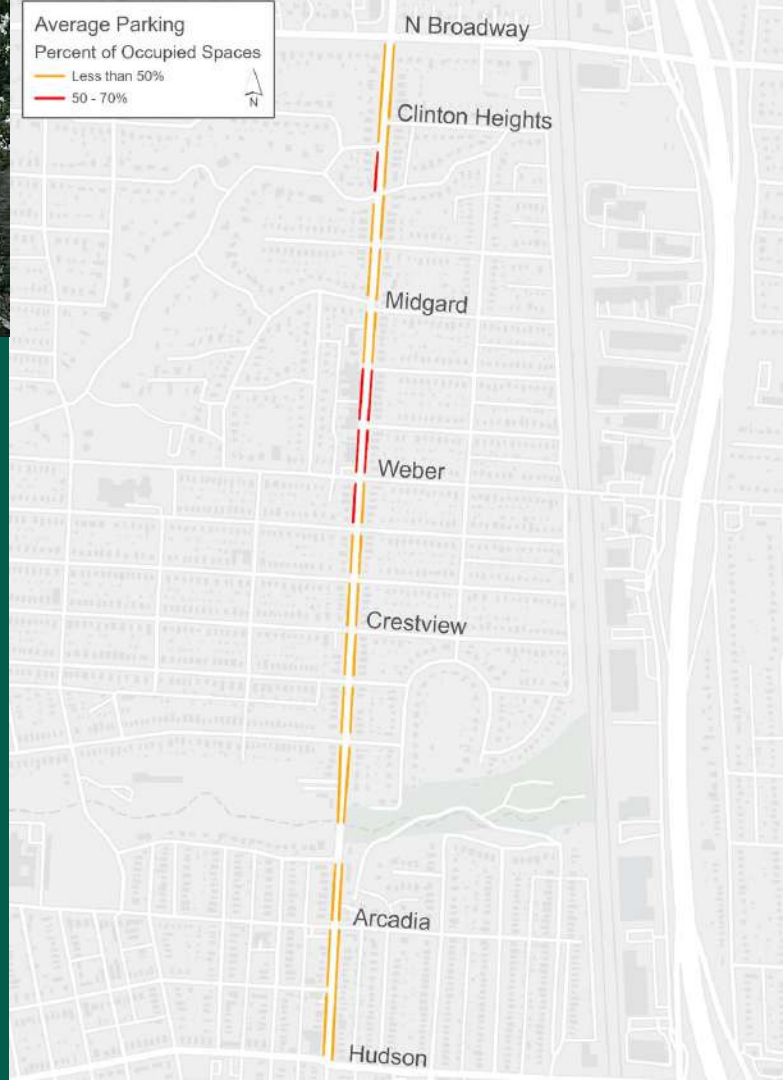
# Safety Review

- High Injury Network
  - Section of study area from Melrose/Milford to Oakland Park
- 7 vulnerable user crashes (2015-2019)
  - 2 serious injury crashes
  - 2 crashes involving pedestrians
  - 2 crashes involving bicyclists
  - 2 crashes involving motorcycles
  - 1 vehicle crash with serious injuries



# Parking Study

- Peak hour restrictions →
- Data collection overview
  - 2pm and 7pm on Friday 9/25/20
  - 2pm and 7pm on Saturday 9/26/20
  - 10pm on Wednesday 4/14/21
- Parking Inventory
  - 299 total spaces
  - 83 spaces being used during highest observed
  - Utilization under 50% for majority of corridor (see map)





# Crosswalk Recommendations

Initial findings support marked crosswalks at the following locations:

- Walhalla Rd\*
- Milford Ave\*
- Melrose Ave\*
- Crestview Rd
- Cliffside Dr

\*Pending COTA bus stop relocation



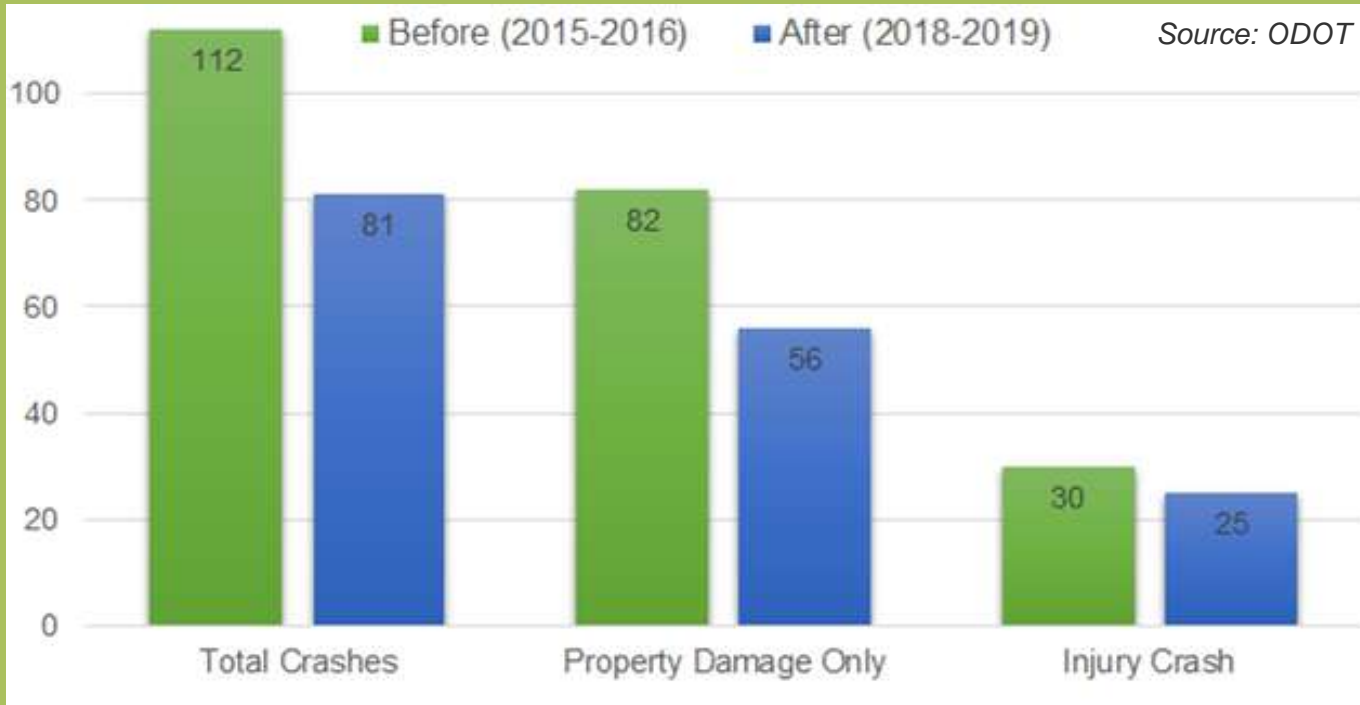
# Capacity Analysis Results

- Corridor-wide results
  - Supports 3 lane configuration along each segment and at the Weber, Arcadia intersections
- North Broadway
  - Intersection is a capacity constraint
  - Columbus staff investigating alternative lane configuration on Indianola Ave approaches within existing pavement width
- Hudson St
  - Initial results show EB lane reduction not feasible
  - Removed from consideration with this study



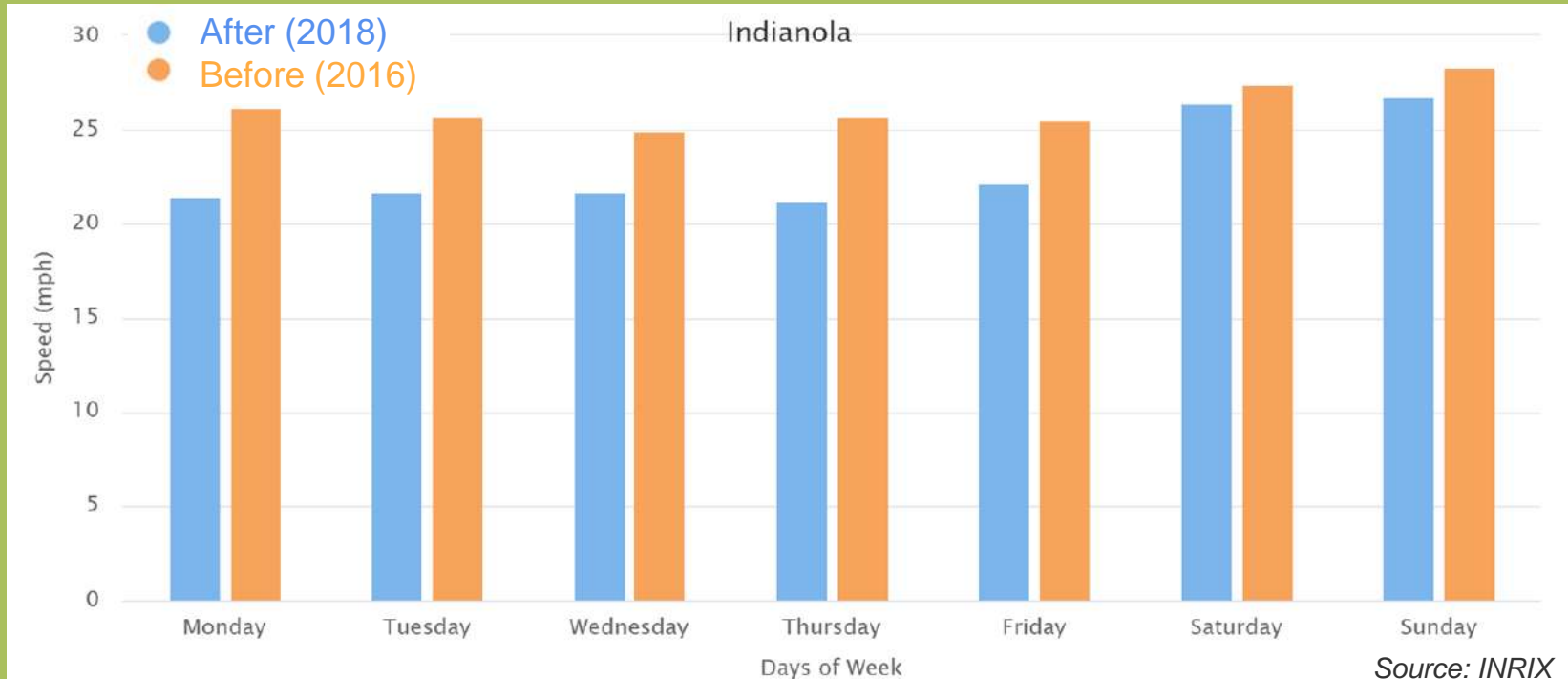
# Lane Reallocation Results: Crash History

- Indianola Avenue from Oakland Park Ave. to Morse Rd.
- Before (2015-2016) and After (2018-2019) crash history



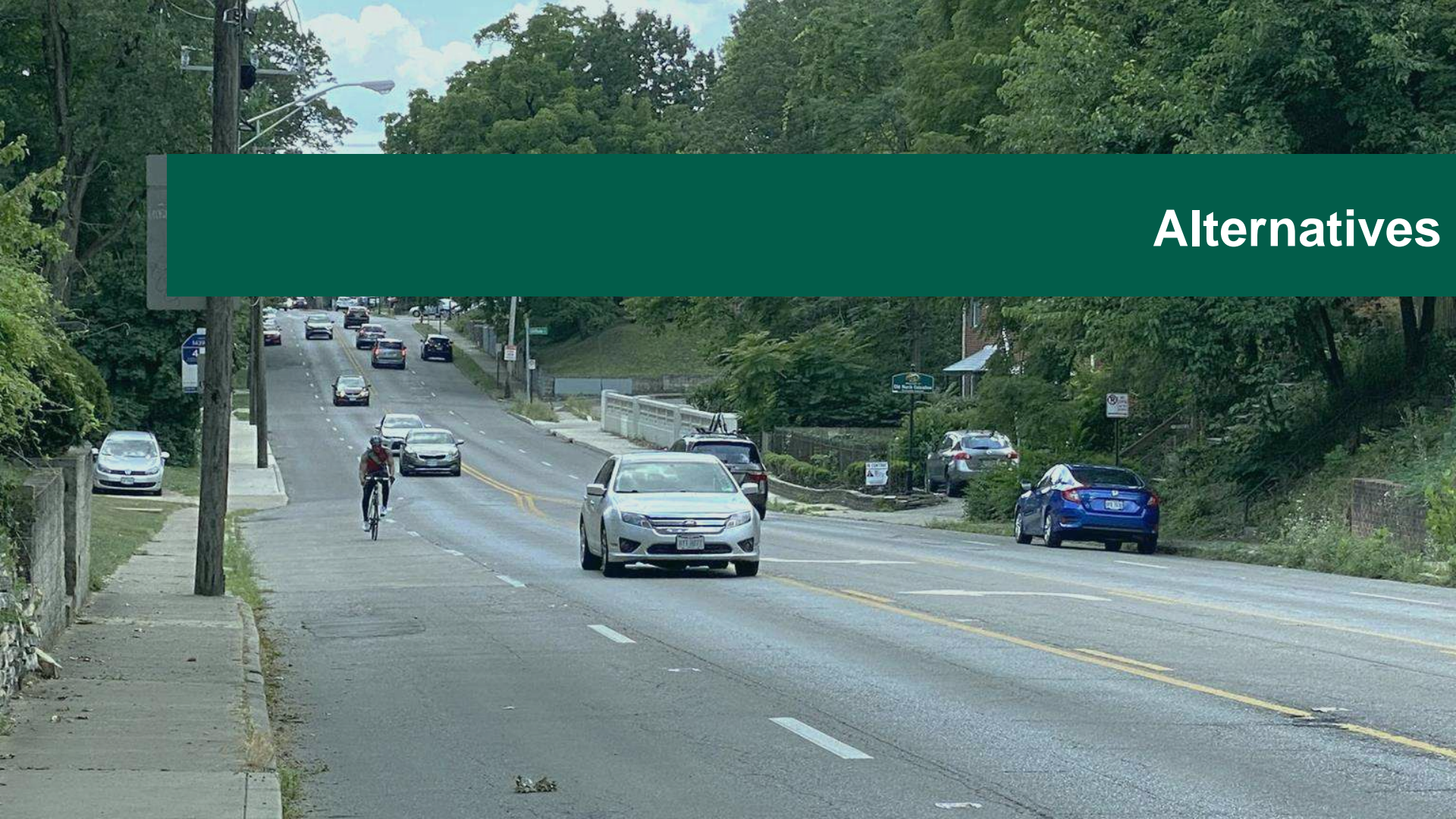
# Lane Reallocation Results: Travel Speeds

- Indianola Avenue from Oakland Park Ave. to Morse Rd.
- Before and after space mean speed (entire day – 24 hours)





# Alternatives



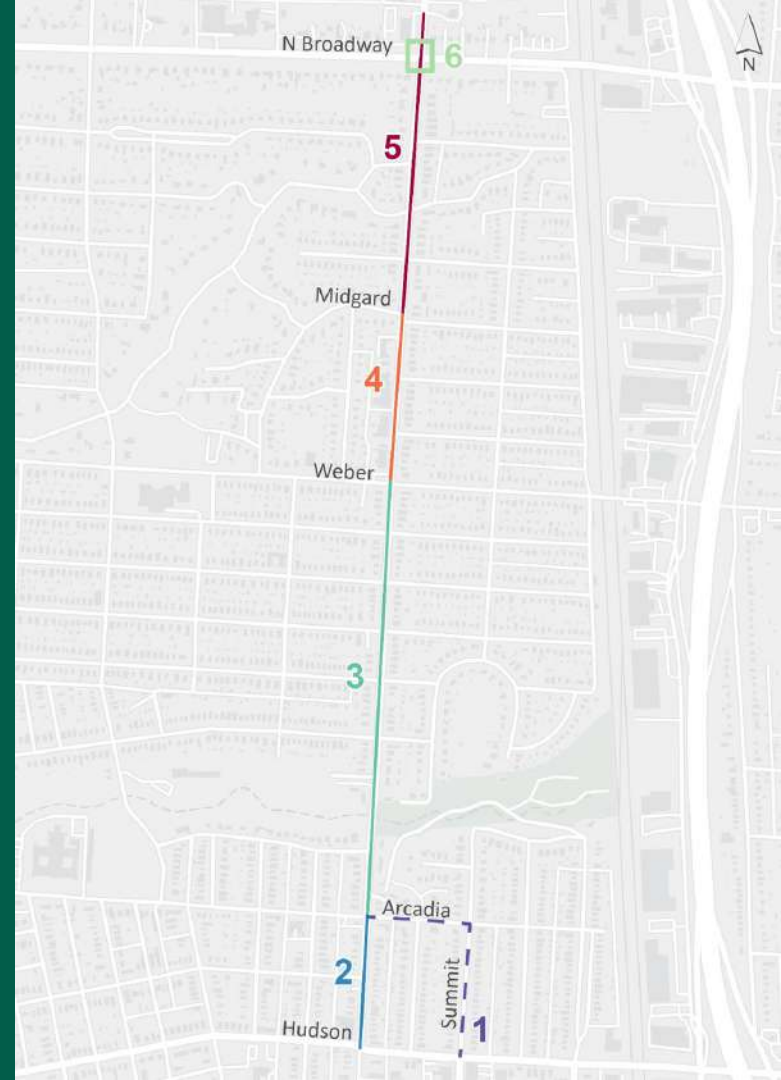


# Alternatives Analysis

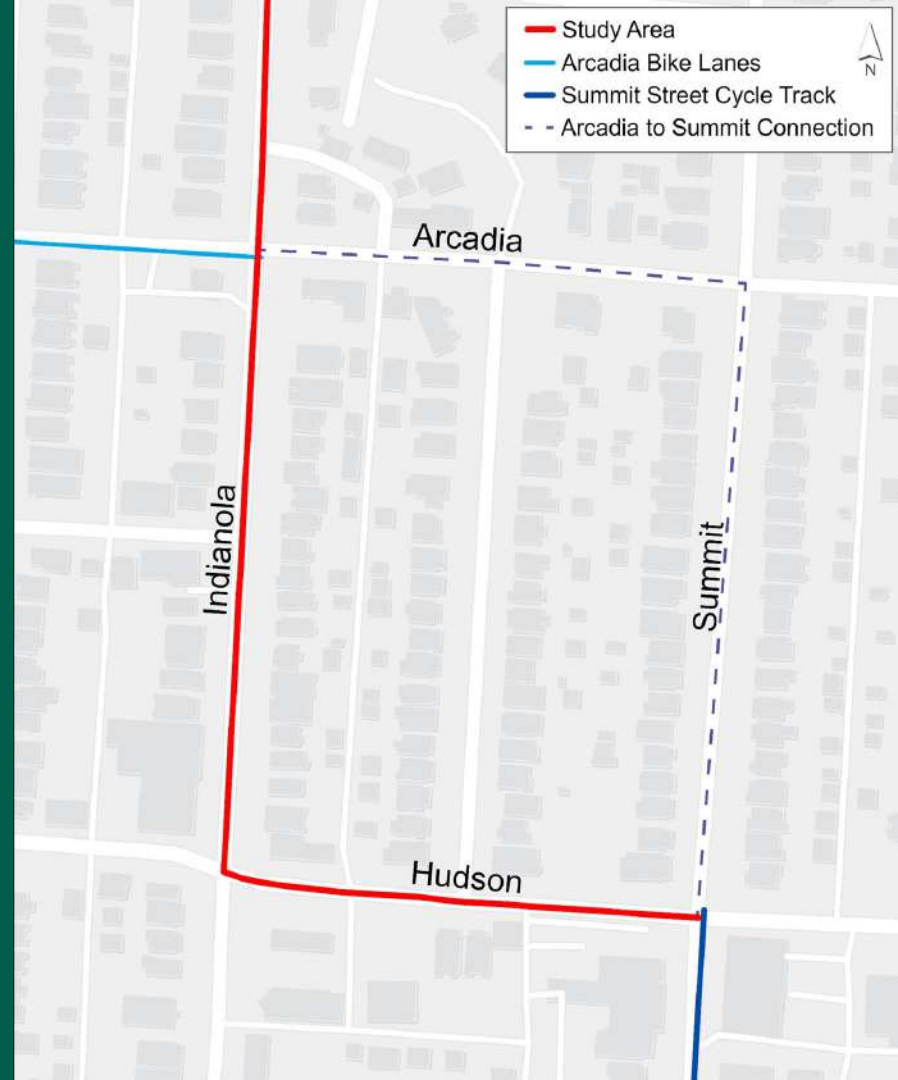
- Alt 1a. No build
- Alt 1b. Remove peak parking restriction; parking on both sides
- Alt 2. Two-way cycle track with parking removed
- Alt 3. Buffered bike lanes with parking removed
- Alt 4. Bike lanes with parking

# Alt 4. Bike Lanes with Parking

1. Summit to Arcadia
  - Bikeway Connection to Cycle Track
1. Hudson to Arcadia
  - Parking on East Side
1. Arcadia to Weber
  - Bike Lanes with Parking on West Side
1. Weber to Midgard
  - Sharrows and Parking on Both Sides
1. Midgard to N Broadway
  - Bike Lanes with Parking on East Side
1. N Broadway to Oakland Park



# Alt 4. Arcadia to Summit Connection







# Shared Bicycle Contraflow Streets



Vancouver, BC



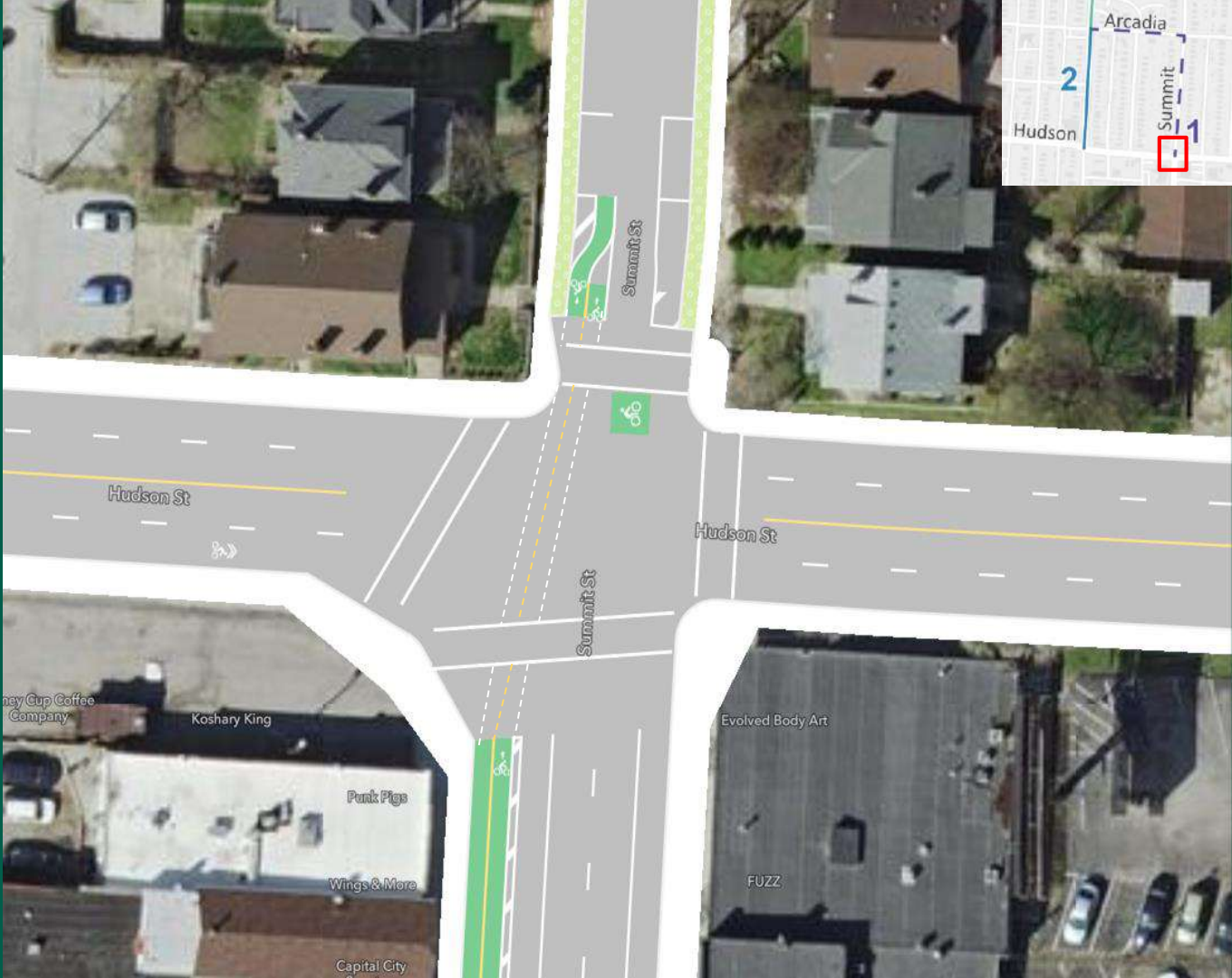
Hancock St. Somerville, MA



Milvia St. Berkeley, CA



# Alt 4. Summit St. Connection



*Note: These images are for illustrative purposes only. Final pavement markings will be determined in design.*

nsy Cup Coffee Company

Koshary King

Pink Figs

Wings & More

Capital City

Evolved Body Art

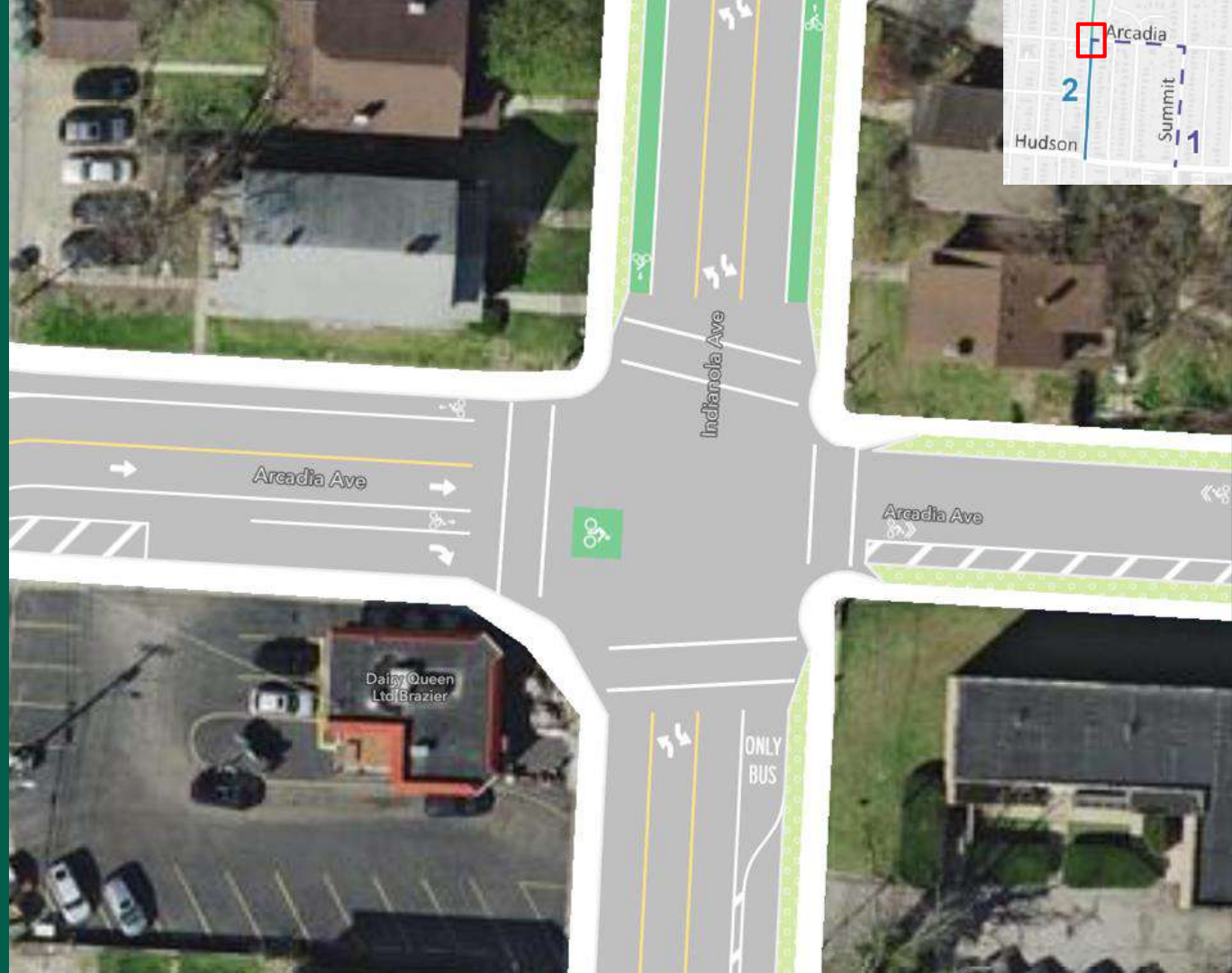
FUZZ

# Existing Conditions: Arcadia Intersection



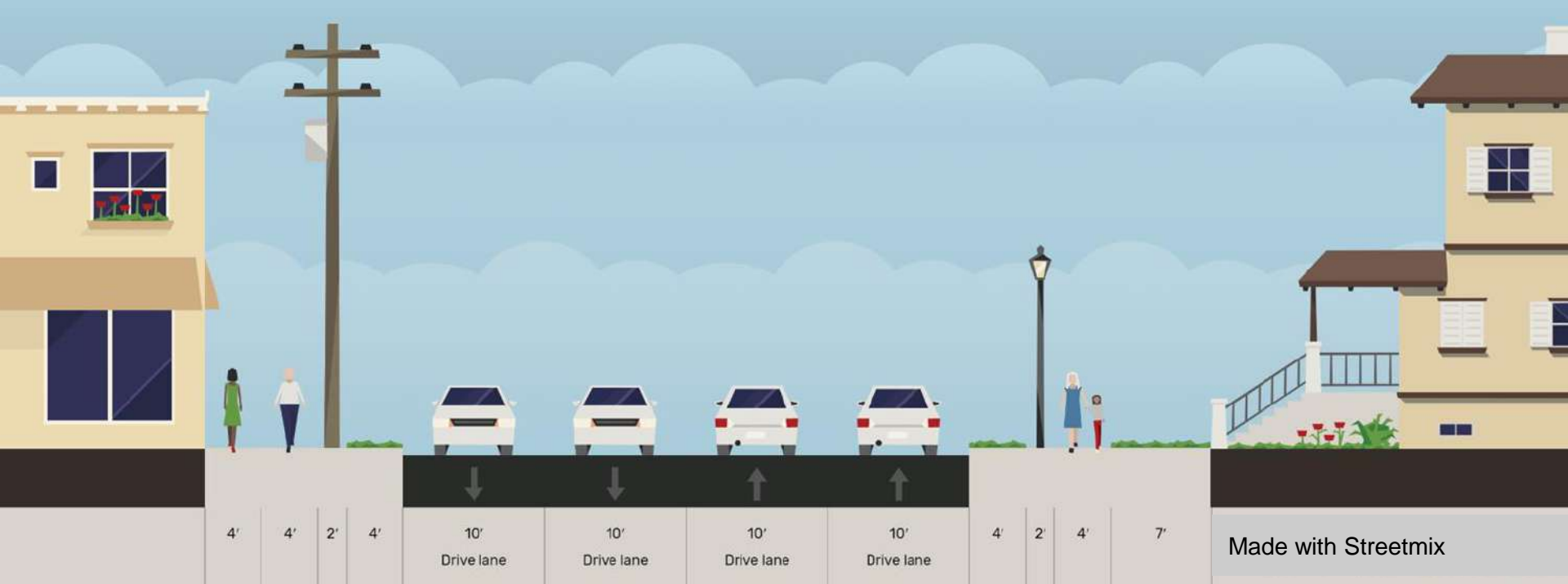


# Alt 4. Arcadia Transition



*Note: These images are for illustrative purposes only. Final pavement markings will be determined in design.*

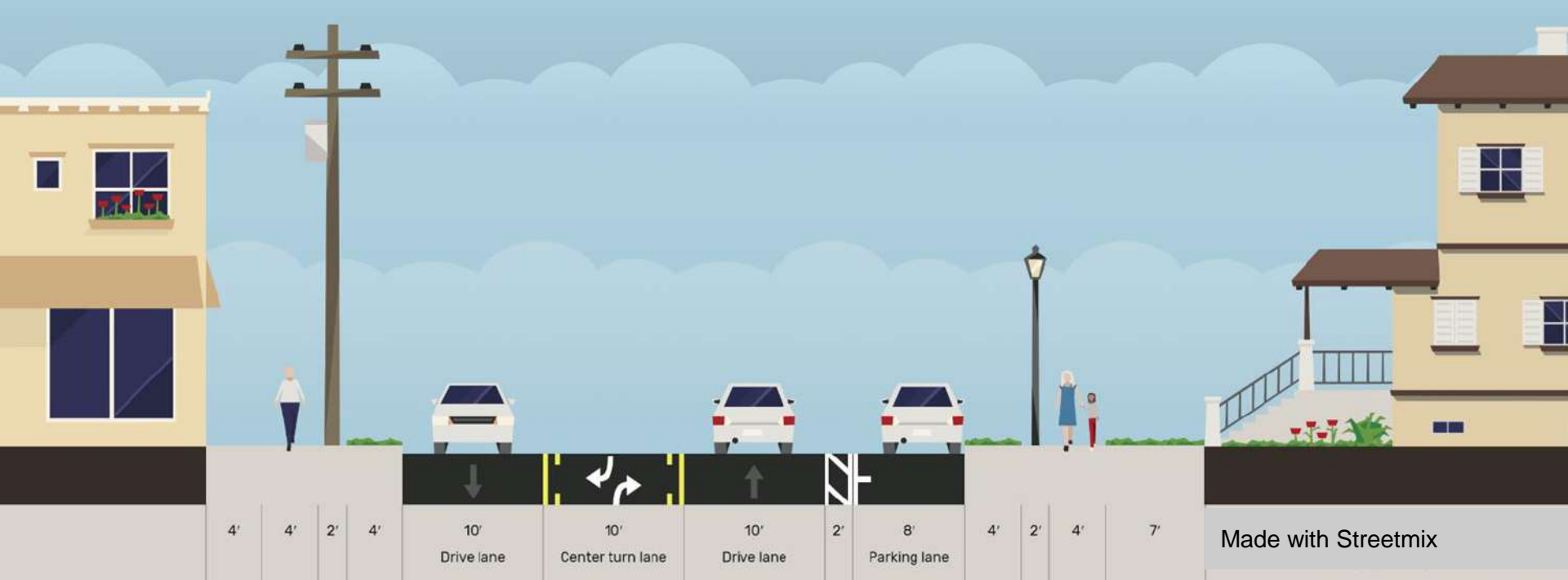
# Existing Conditions: Hudson to Duncan



Made with Streetmix

*Note: These images are for illustrative purposes only. Dimensions are preliminary and are under review.*

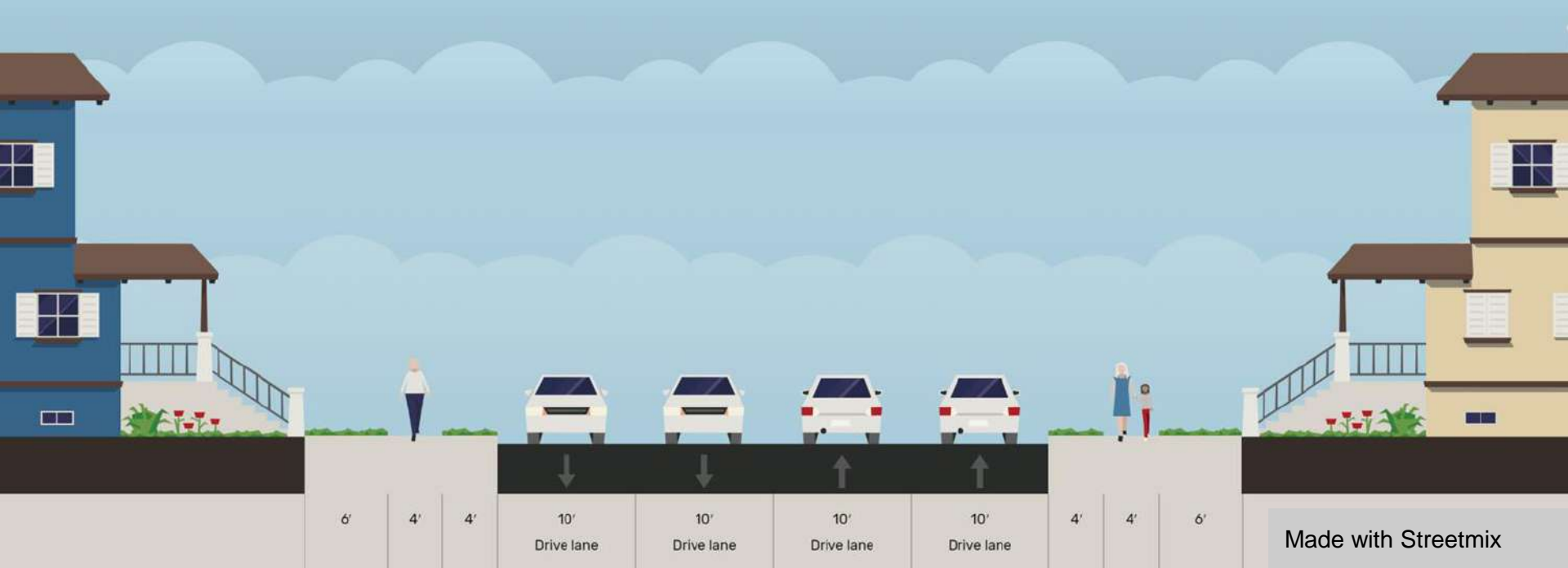
# Alt 4. Hudson to Duncan



*Note: These images are for illustrative purposes only. Dimensions are preliminary and are under review.*



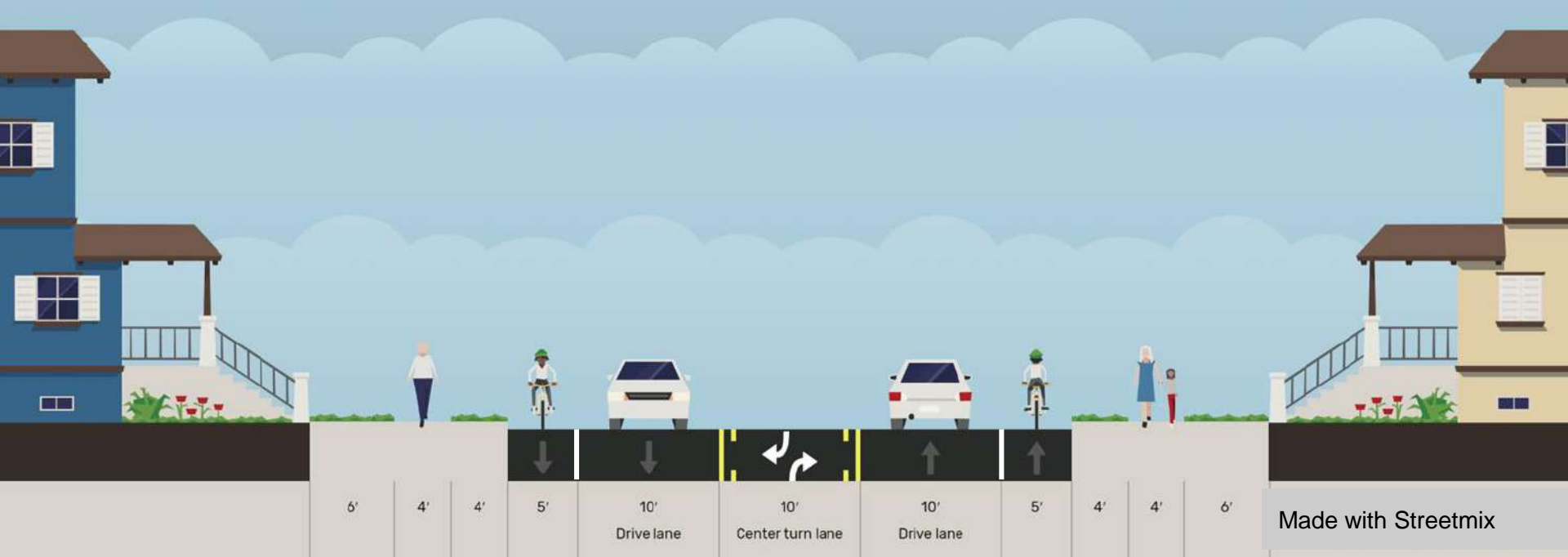
# Existing Conditions: Arcadia to Cliffside



Made with Streetmix

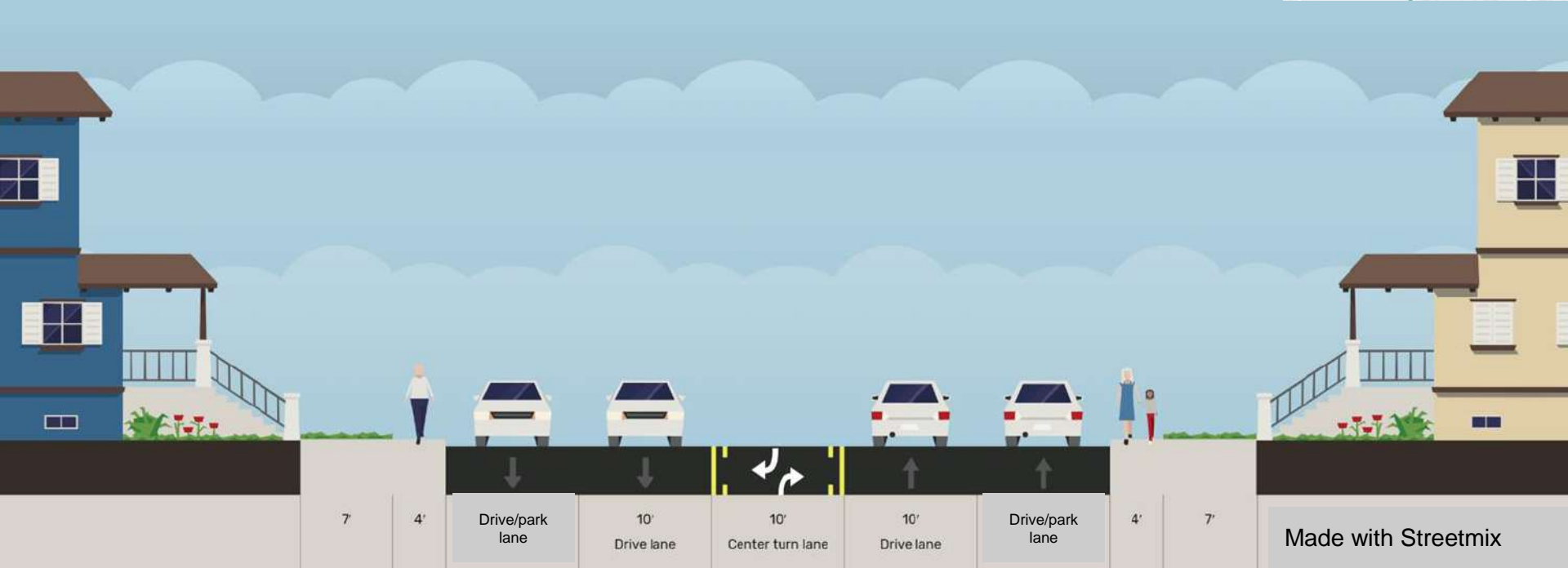
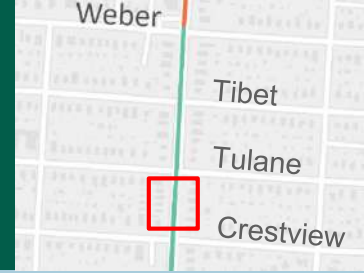
*Note: These images are for illustrative purposes only. Dimensions are preliminary and are under review.*

# Alt 4. Arcadia to Cliffside



*Note: These images are for illustrative purposes only. Dimensions are preliminary and are under review.*

# Existing Conditions: Crestview to Tulane



*Note: These images are for illustrative purposes only. Dimensions are preliminary and are under review.*

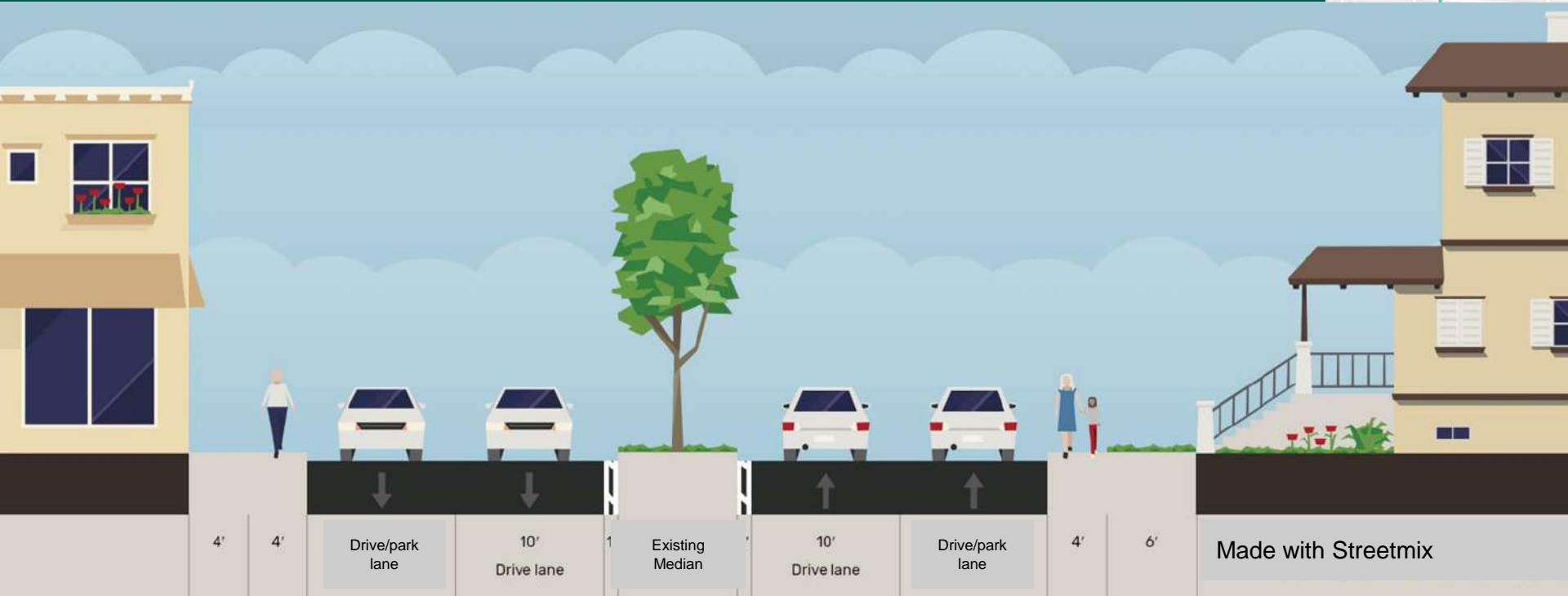
# Alt 4. Crestview to Tulane



*Note: These images are for illustrative purposes only. Dimensions are preliminary and are under review.*

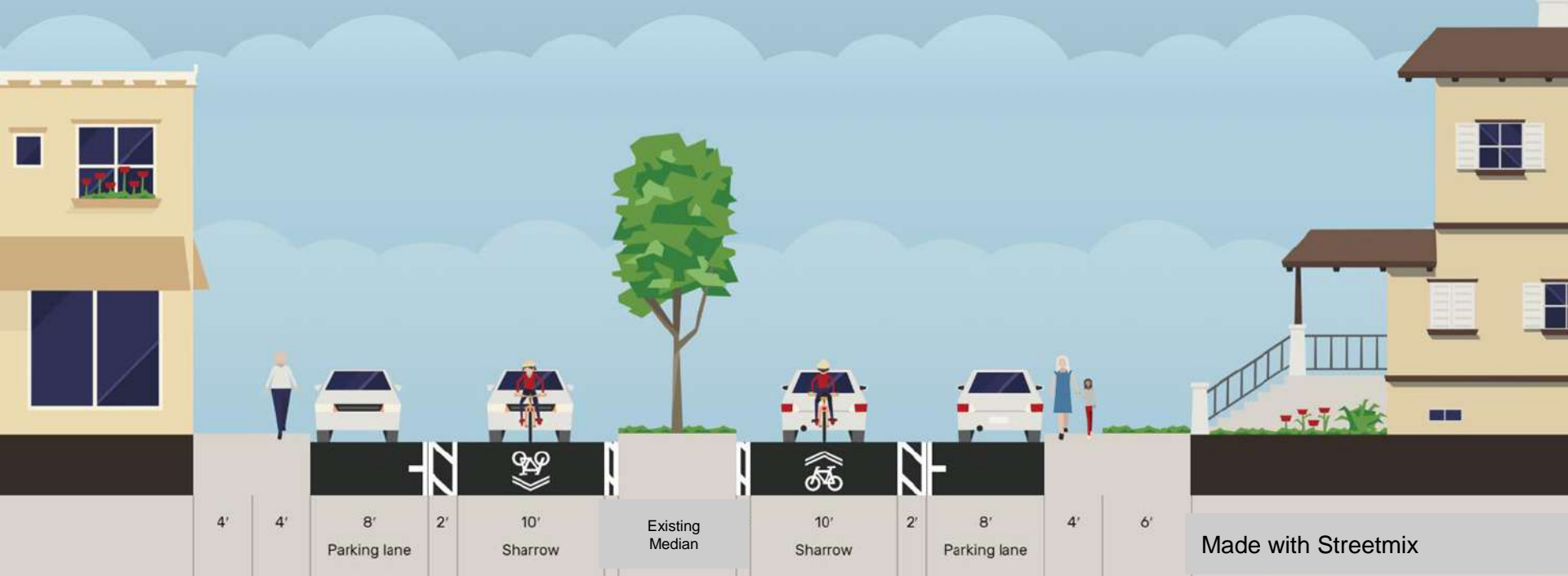


# Existing Conditions: Melrose to Milford



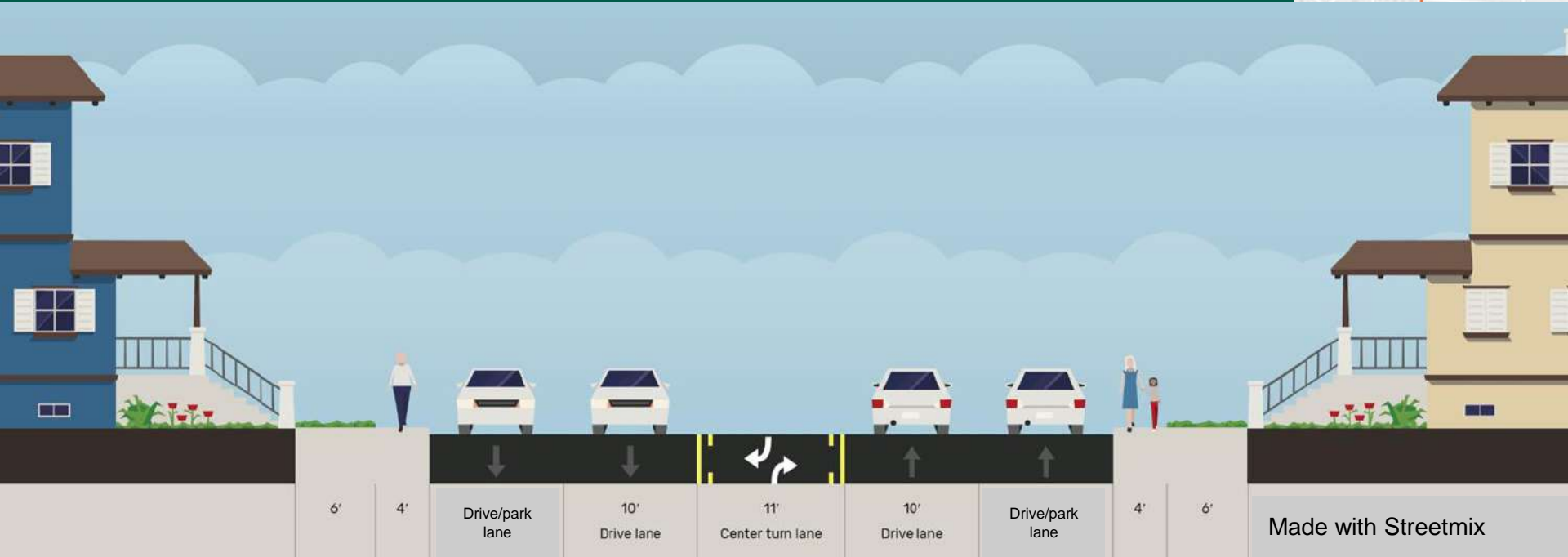
*Note: These images are for illustrative purposes only. Dimensions are preliminary and are under review.*

# Alt 4. Melrose to Milford



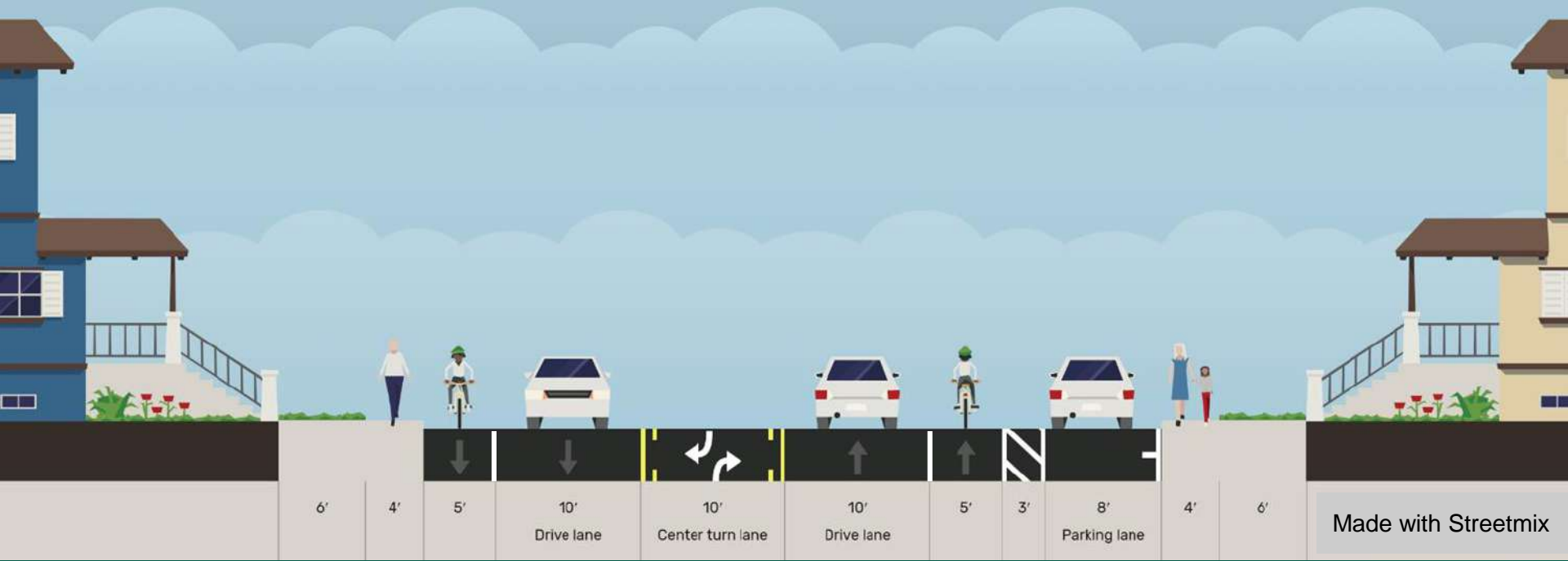
Note: These images are for illustrative purposes only. Dimensions are preliminary and are under review.

# Existing Conditions: Como to Walhalla



*Note: These images are for illustrative purposes only. Dimensions are preliminary and are under review.*

# Alt 4. Como to Walhalla



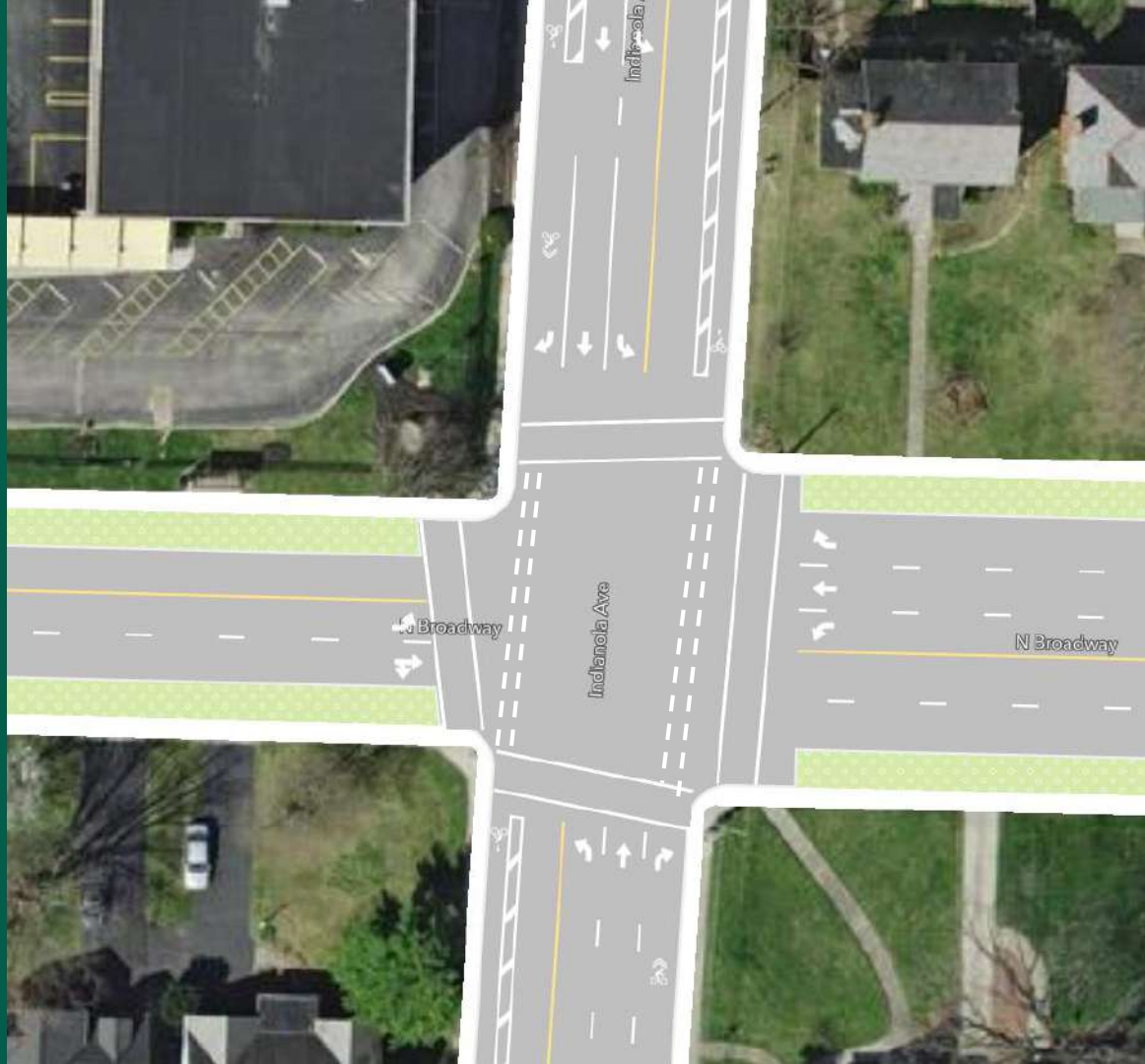
*Note: These images are for illustrative purposes only. Dimensions are preliminary and are under review.*

Existing Conditions:  
N Broadway  
Intersection





# Alt 4. N Broadway Intersection



*Note: These images are for illustrative purposes only.  
Final pavement markings will be determined in design.*

# Next Steps

- **Public comment period until October 1st**
  - Review materials and provide comments at [tinyurl.com/cbusindianola](https://tinyurl.com/cbusindianola)
  - Or send comments and questions to [Indianola@columbus.gov](mailto:Indianola@columbus.gov)
- Finalize the corridor plan
- Design alternative in alignment with urban repaving project

# Q&A

Type questions in the chat

Use the “Raise Hand” function

Send your more detailed questions after the meeting to

[Indianola@columbus.gov](mailto:Indianola@columbus.gov)

# Thank You for Joining Us



THE CITY OF  
**COLUMBUS**  
ANDREW J. GINTHER, MAYOR

DEPARTMENT OF  
PUBLIC SERVICE

## INDIANOLA AVENUE COMPLETE STREET STUDY

Send your questions and comments to:

[Indianola@columbus.gov](mailto:Indianola@columbus.gov)



## **Appendix C. University Area Commission Letter for Hudson Street Corridor Improvements**

A letter was received by the University Area Commission on September 16, 2021 in support of traffic calming and safety improvements along the Hudson Street Corridor.





City of Columbus  
Mayor Andrew J. Ginther

## University Area Commission

Northwood High Building  
2231 North High Street  
Columbus, Ohio 43201  
(614) 441-8174  
[www.universityarea.org](http://www.universityarea.org)

*Serving the University Community for over 30 Years*

Doreen Uhas-Sauer  
President  
Lauren Squires  
1<sup>st</sup> Jeremy Gabis  
David Hegley  
2<sup>nd</sup> Vice President  
Stephen Waldron  
Corr. Secretary  
Daniel Snider  
Recording Secretary  
Seth Golding  
Treasurer

Dear Mayor Andrew Ginther,  
Columbus City Council,  
Division of Traffic Management:

As you may be aware, the Indianola Avenue Complete Streets study is underway and extends on Indianola Avenue from Hudson Street to Oakland Park Avenue. According to the project committee, this project is being completed in conjunction with a planned resurfacing project and in partnership with the Ohio Department of Transportation. Construction is set to begin around late 2023.

*District 1:*

Andrew Hinger  
Daniel Snider  
Lauren Squires

The Indianola Complete Streets project committee included a portion of Hudson Street from Indianola Avenue to Summit Street to assess the connection between the Summit Street cycle track and Indianola Avenue. The committee has determined that attempting to evaluate this section of road for improvements was infeasible without also including a road evaluation of Hudson Street.

*District 2:*

Jenny Bell  
Seth Golding  
Tom Wildman

In 2020, the University Area Commission submitted a capital improvement project proposal targeting safety and traffic calming measures along the Hudson Street corridor. As this year has passed, the need for traffic calming along Hudson Street has become even more apparent. Columbus Vision Zero categorizes the area around Hudson and Indianola as being a "high injury" corridor. Many pedestrian, bicycle and vehicle crashes and near misses have been reported in this year alone.

*District 3:*

Davide Cugini  
Michael Wilkos  
Brian Williams

In short, we have reached a crossroads of urgency and opportunity. The Indianola Complete Streets project committee's work is already underway. It is critical for the Indianola Complete Streets project committee to plan their work with a system view of the transportation network and act with the necessary data. Accordingly, the University Area Commission asks City Council to prioritize and expedite the Hudson Corridor Traffic Calming capital improvement proposal.

*District 4:*

Michael Kehlmeier  
Timothy Sublette  
Steve Waldron

*Appointees:*

Joshua Badzik  
Jeremy Gabis  
Debra Gallagher  
Justin Garland  
Pasquale Grado  
David Hegley  
Nick Messenger  
Doreen Uhas-Sauer

Thank you for your attention.

University Area Commission